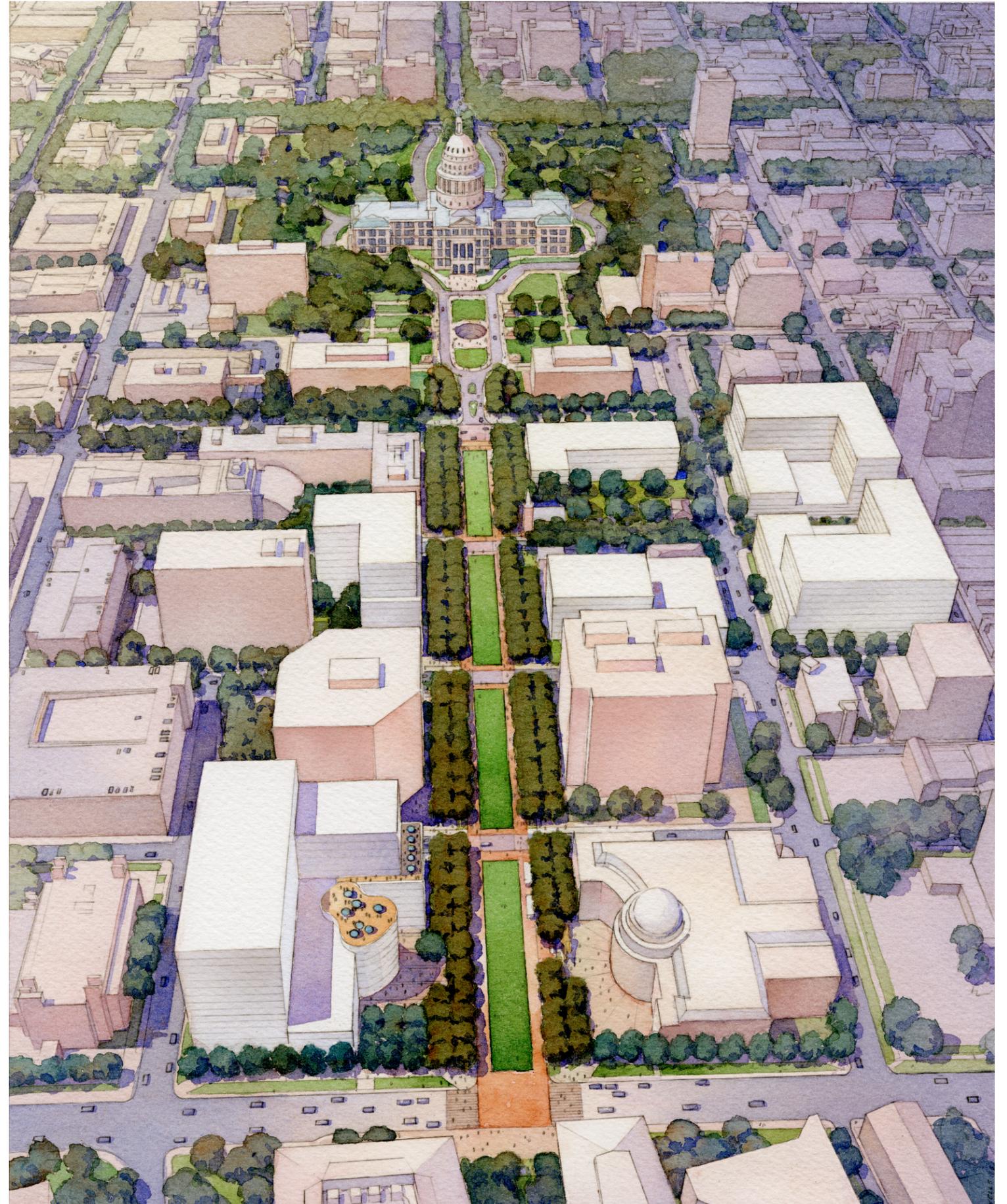




**Texas
Capitol Complex
Master Plan Phase 1-3
Proof-of-Concept Study**





Conceptual rendering of the Texas Mall.

Courtesy of Timothy Wells

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Introduction

1 Introduction

Following the adoption of the 2016 Texas Capitol Complex Master Plan, the Texas Facilities Commission engaged Page to develop a proof-of-concept study for Phases 1 through 3 of the proposed development associated with the new Texas Mall. This study provides a level of detail between the master plan and detailed building, mall, and associated landscape design as a method of testing the master plan elements.

The Page team utilized the proposed building massing, garage configuration, and landscape design guidelines to create floor plans and landscape layouts. These layouts identified key elements, such as loading, parking entrances, egress points, and landscape features, to determine how they will impact or be impacted by existing site components. The following pages provide a set of technical drawings and notes to inform a more detailed design process. The included drawings represent one potential high-level solution but are not reflective of a singular solution for the ultimate design.

Additionally, meetings were held with various stakeholders along the future Texas Mall to determine concerns or constraints with the proposed development. Major themes from those meetings are included for reference by the design teams.







Conceptual Architectural Drawings

2 Conceptual Architectural Drawings

The architecture of Phases 1-3 of the Capitol Complex Master Plan includes six new buildings to be used primarily for State office functions. Each individual phase includes two buildings. All building sites are designated by an adjacent street in the existing condition. Phase 1 sites are known as MLK and Congress; Phase 2 sites are 15th Street and Lavaca; Phase 3 sites are 17th Street and Colorado.

Each building site includes associated parking for the building plus additional parking that seeks to replace existing garages within the Capitol Complex that are in poor condition. Four of the sites include both below-grade and above-grade parking, while the Congress and 15th Street sites only include below-grade parking. Phases 1 and 2 include parking beneath the Texas Mall adjacent to the nearest building site. Phase 1 also incorporates a "Mall Connector" parking block beneath the Texas Mall where no immediately adjacent building is constructed.

This study examines the impact of the proposed buildings and parking structures to determine any concerns or challenges with a potential layout. Elements investigated include the building massing, garage layout and access, loading access, egress requirements, below-grade ventilation, and possible building core locations. Only the four buildings adjoining the Texas Mall were studied, as the Lavaca and Colorado sites present a more standard, single-block construction project.

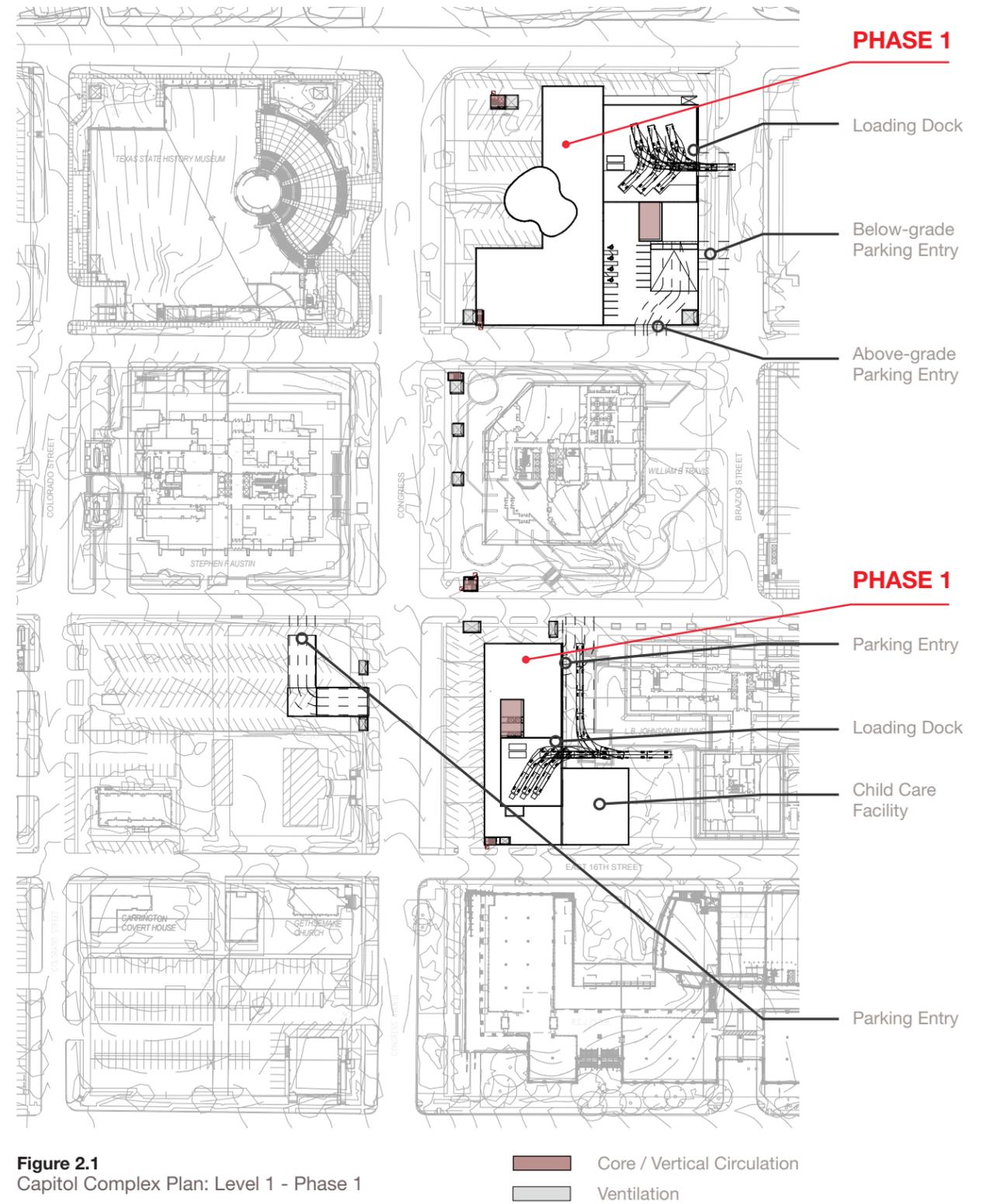


Figure 2.1
Capitol Complex Plan: Level 1 - Phase 1

Core / Vertical Circulation
Ventilation

BUILDING REQUIREMENTS

The following requirements are proposed in the master plan :

MLK site

- 605,000 GSF of building area in 15 stories
- 2,550 parking spaces in 9 above-grade and 5 below-grade parking levels

Congress site

- 420,000 GSF of building area in 14 stories
- 1,000 parking spaces in 5 below-grade parking levels
- Relocated Child Care Facility (CCF)

Mall Connector

- 240 parking spaces in 5 below-grade parking levels

15th Street site

- 165,000 GSF of building area in 5 stories
- 900 parking spaces in 5 below-grade parking levels

17th Street site

- 170,000 GSF of building area in 9 stories
- 840 parking spaces in 6 above-grade and 5 below-grade parking levels

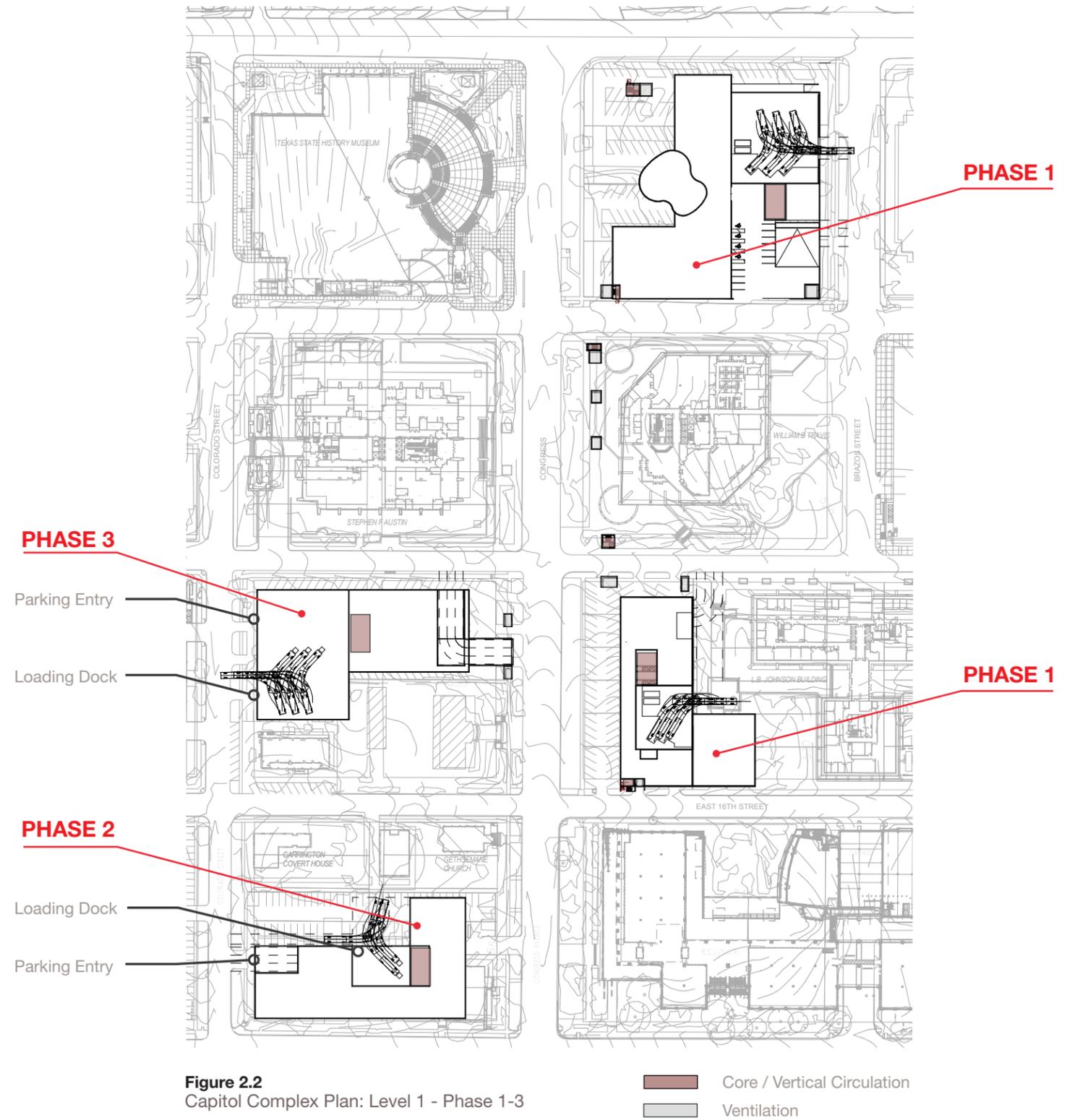


Figure 2.2
Capitol Complex Plan: Level 1 - Phase 1-3

Core / Vertical Circulation
Ventilation

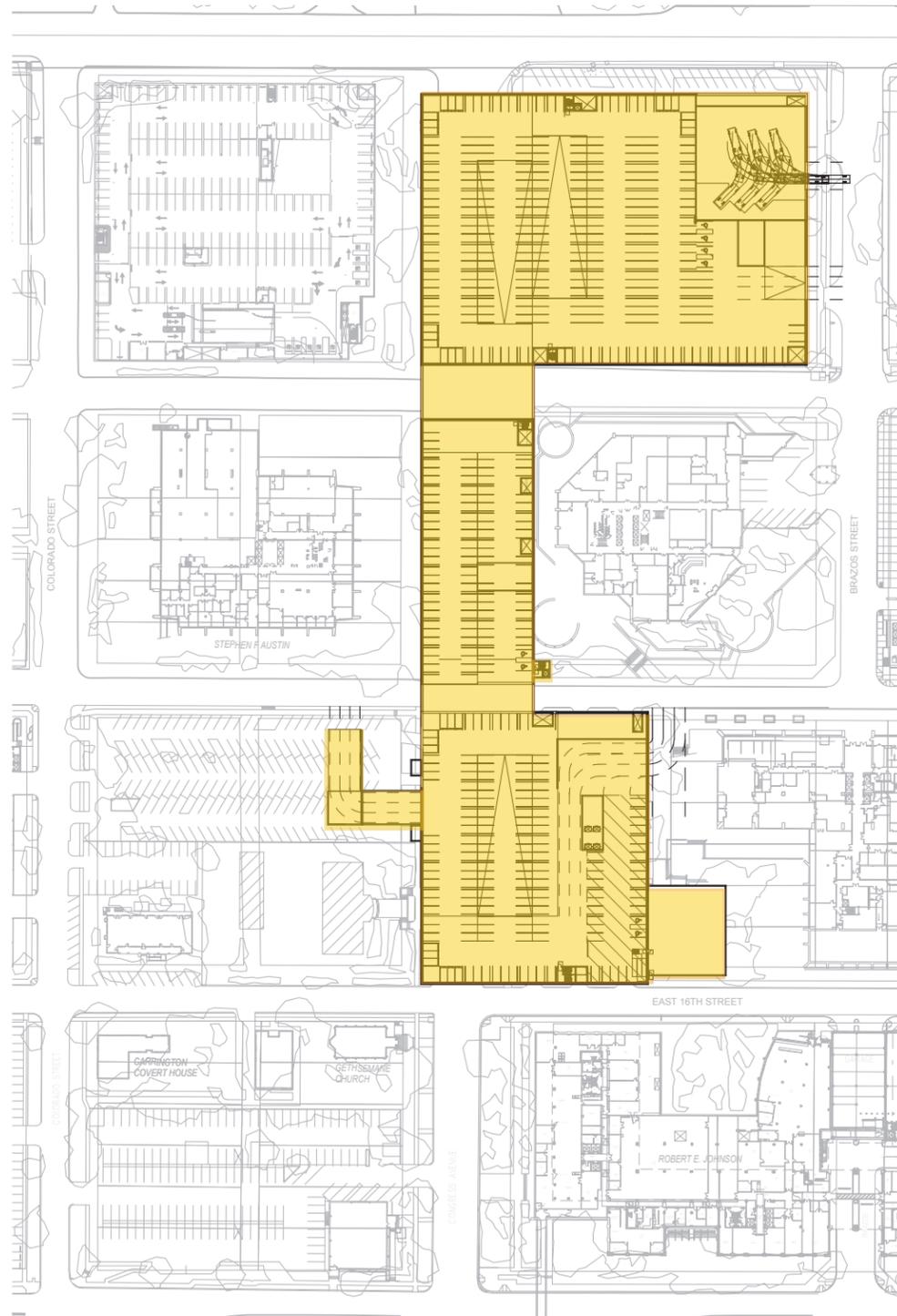


Figure 2.3
Capitol Complex Plan: Garage Level B1 - Phase 1

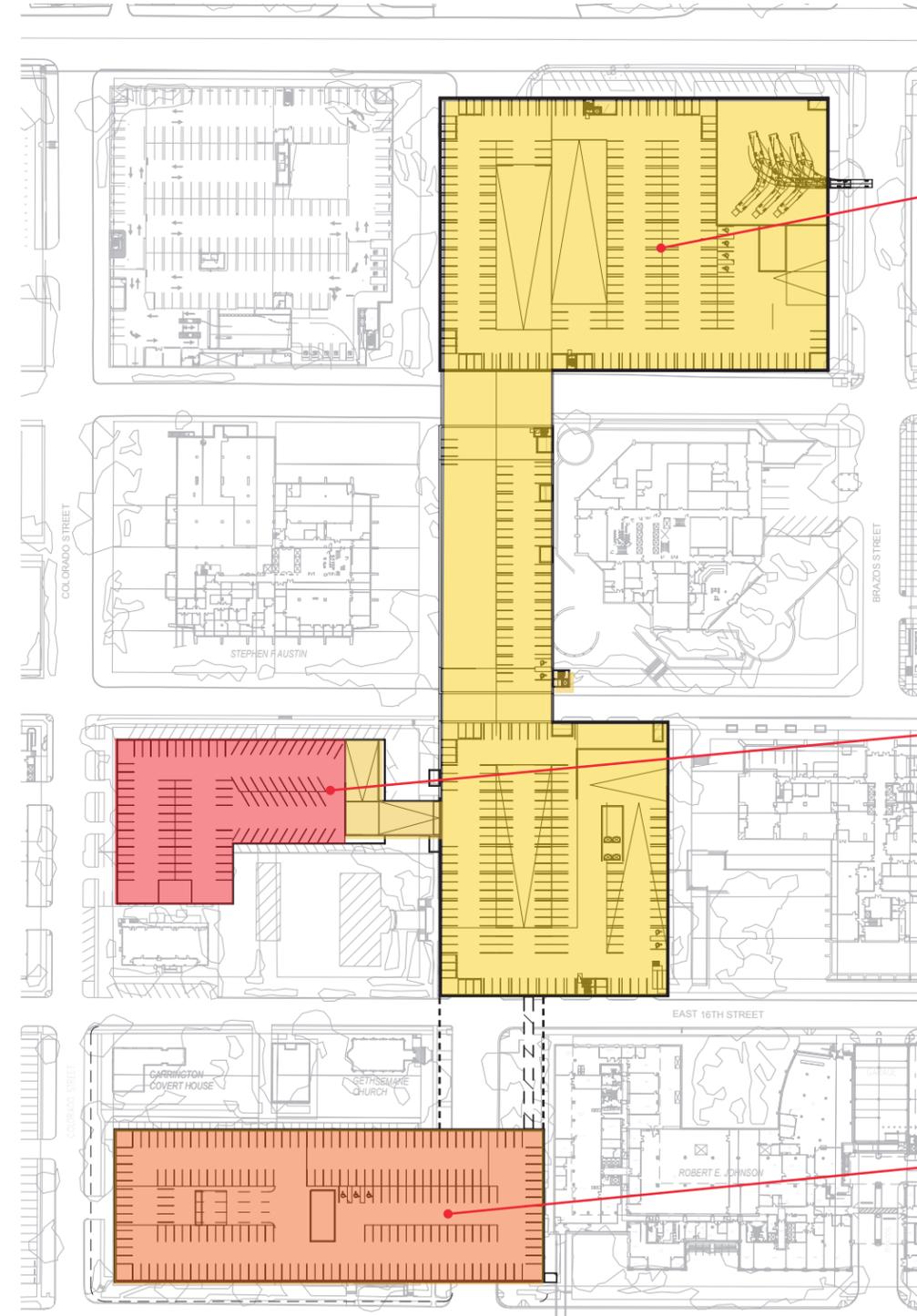


Figure 2.4
Capitol Complex Plan: Garage Level B1 - Phase 1-3

PHASE 1

PHASE 3

PHASE 2

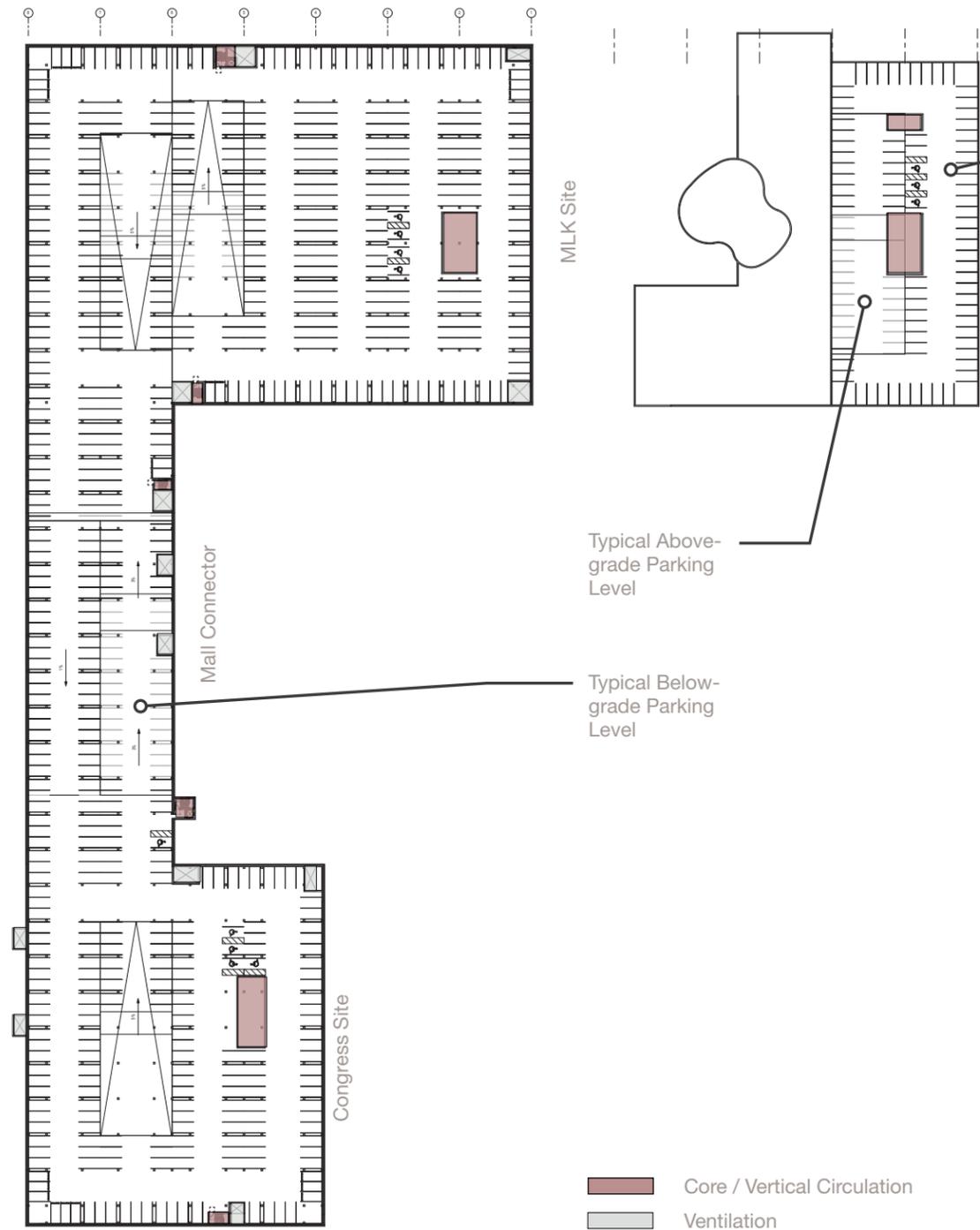


Figure 2.5
Capitol Complex Plan - Typical Below- and Above-Grade Garage Levels - Phase 1

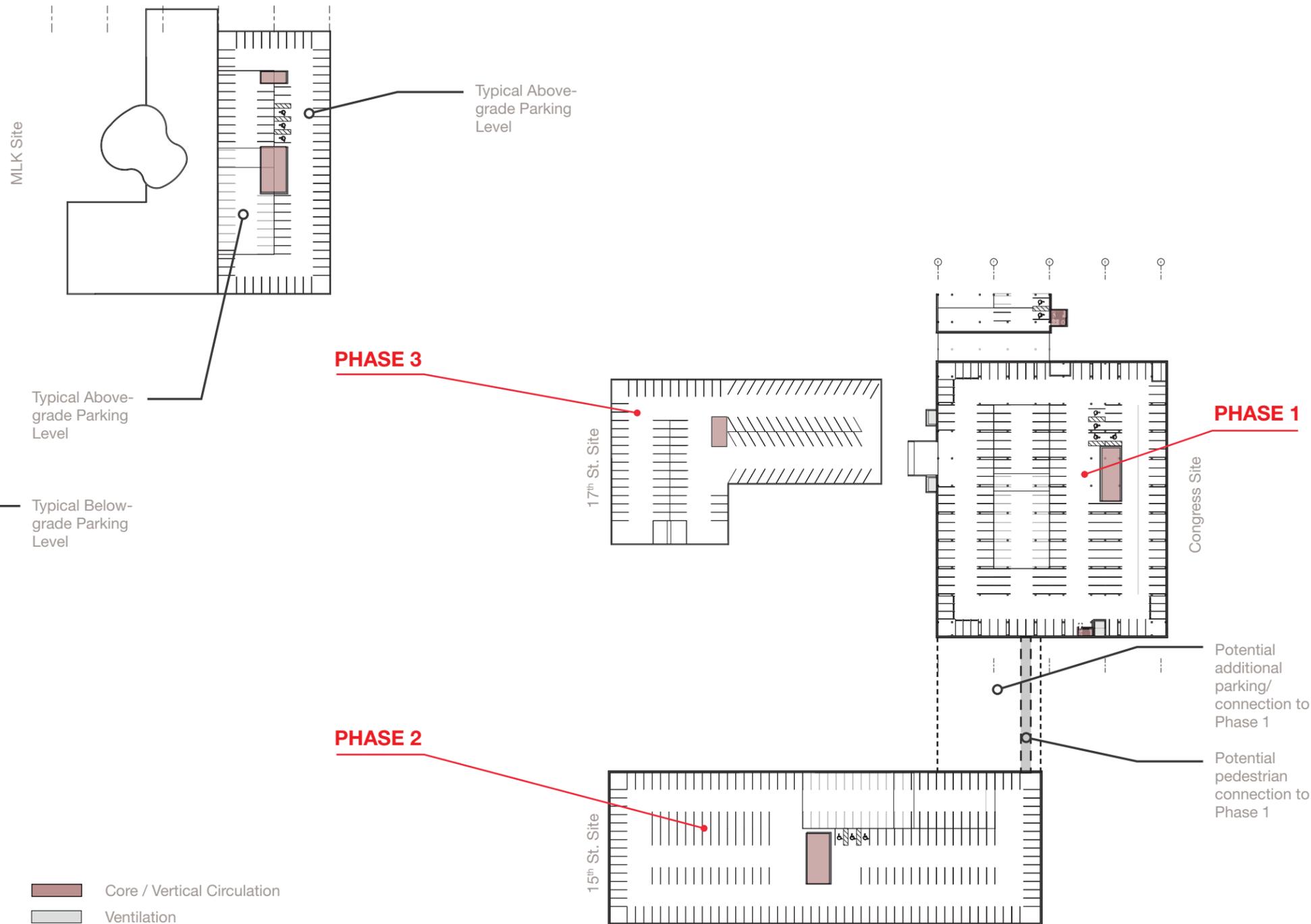


Figure 2.6
Capitol Complex Plan - Typical Garage Level - Phase 2-3

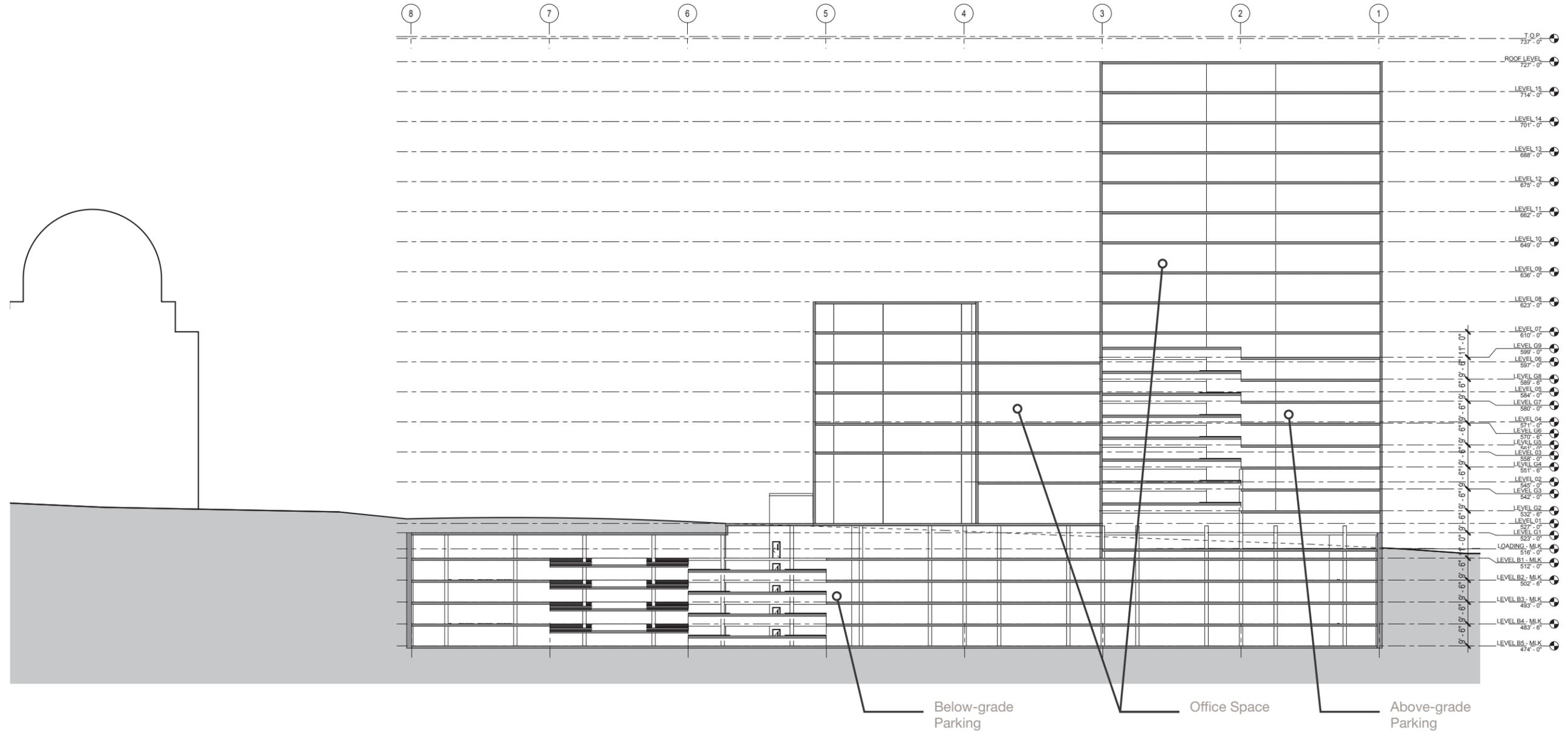


Figure 2.7
Site Section at MLK Building - Phase 1

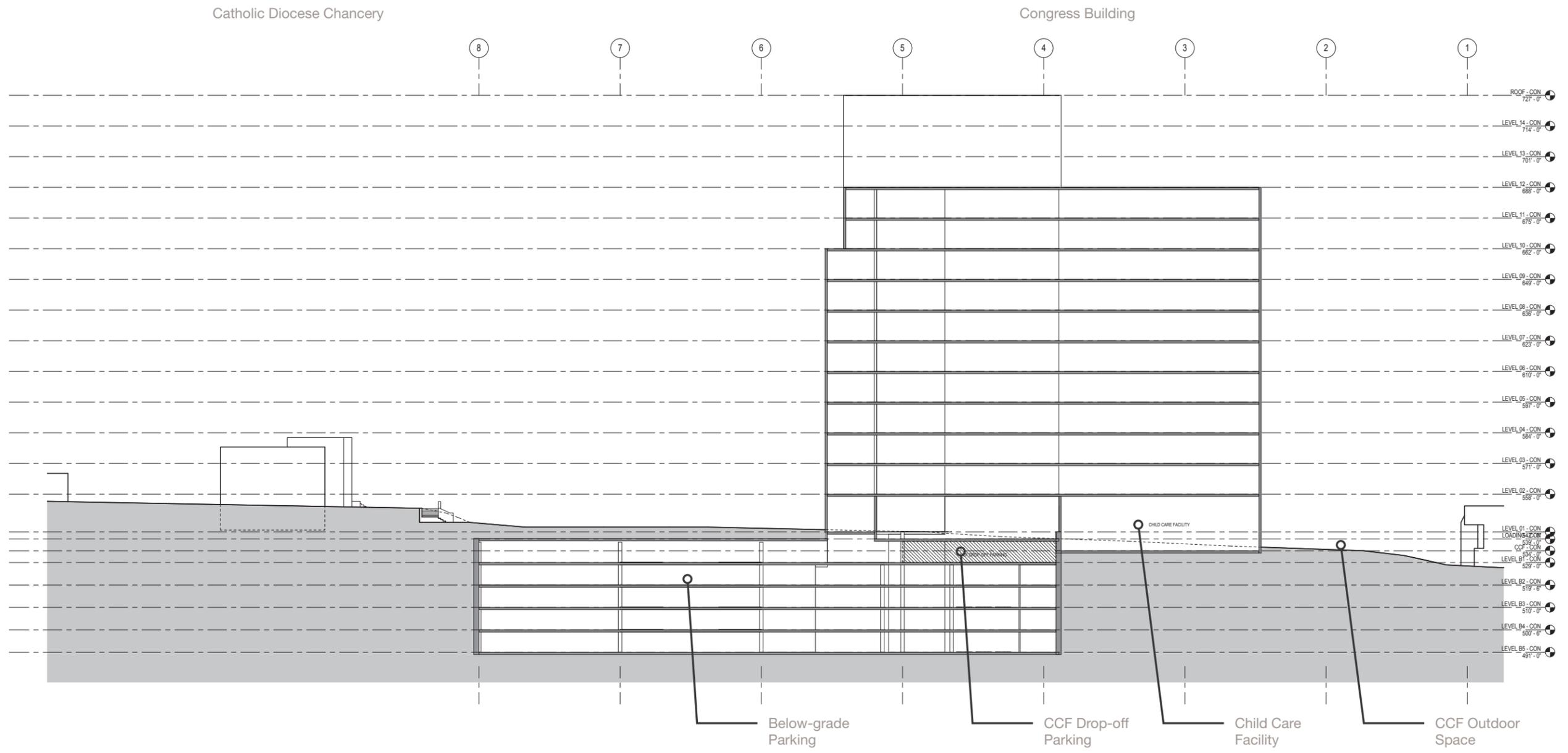


Figure 2.8
Site Section at Congress Building - Phase 1

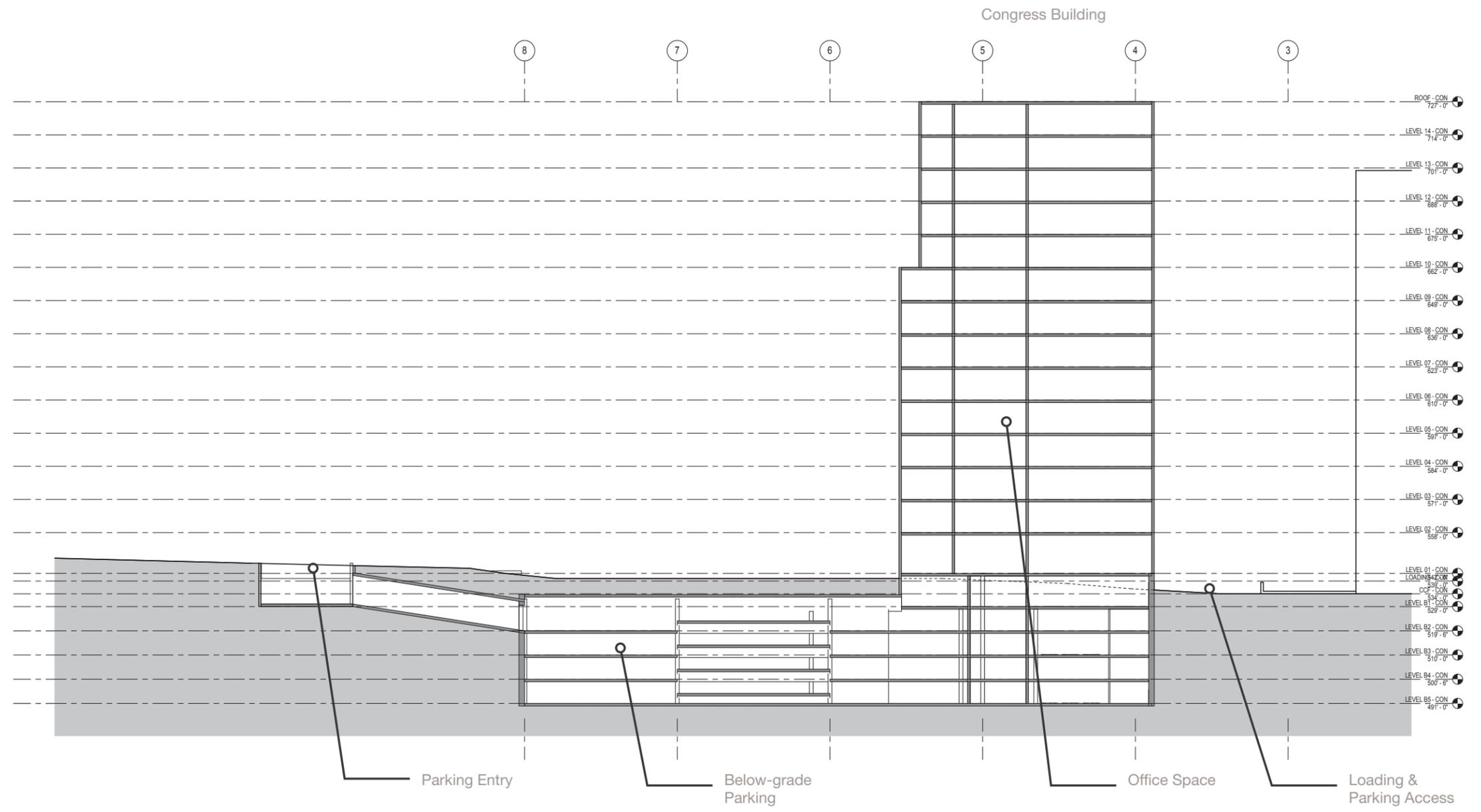


Figure 2.9
Site Section at Congress Building - Phase 1

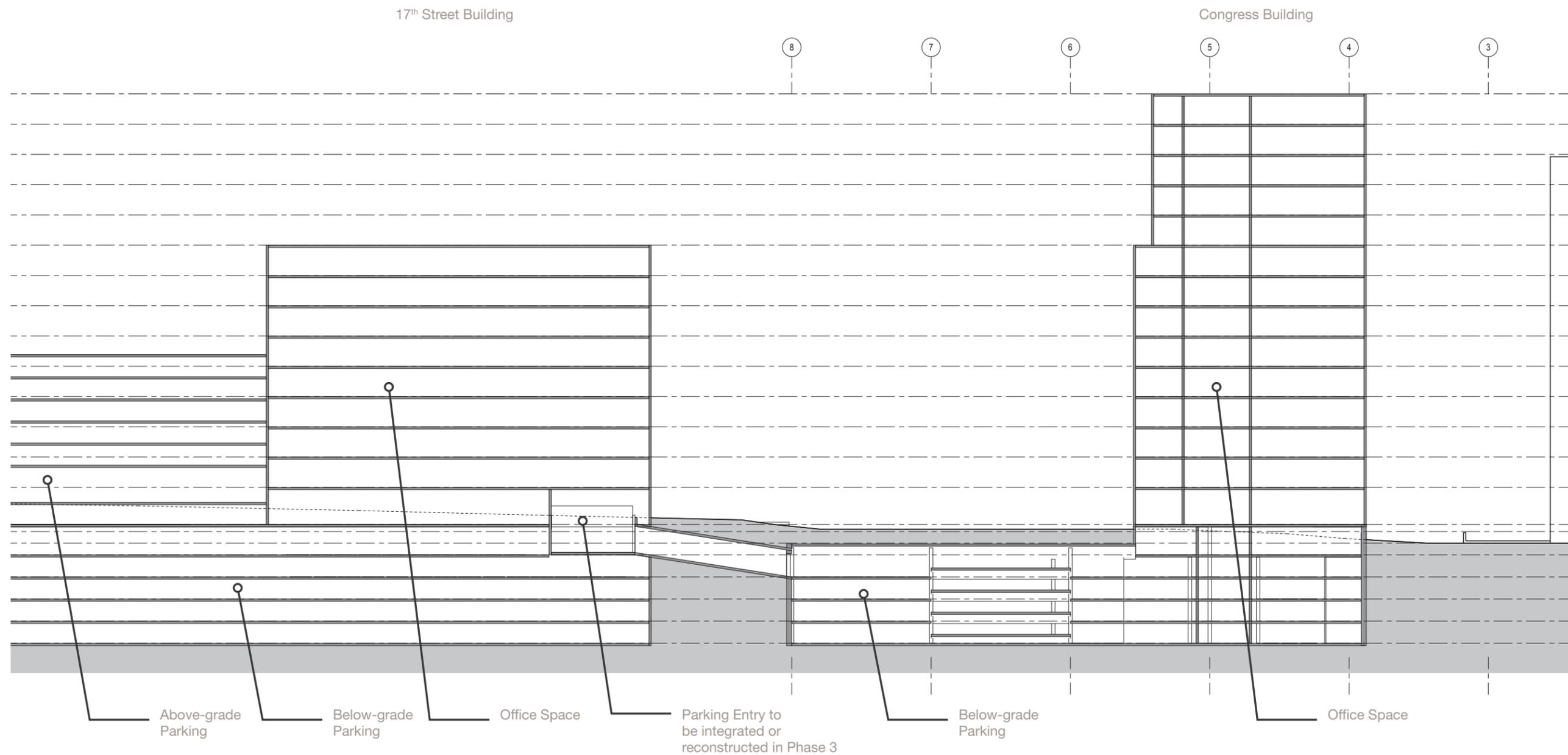


Figure 2.10
 Site Section at Congress Building & 17th Street Building - Phase 1-3

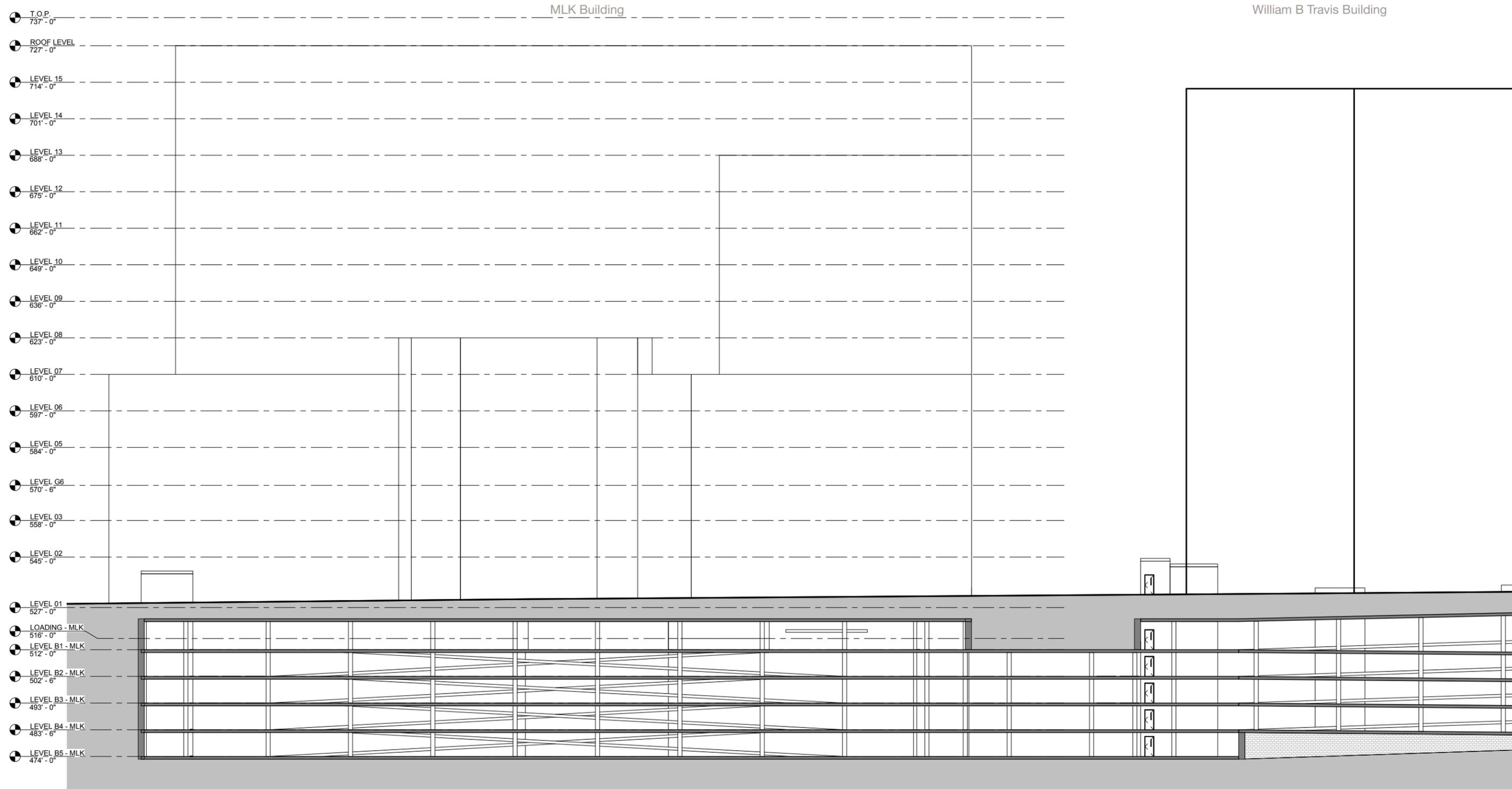
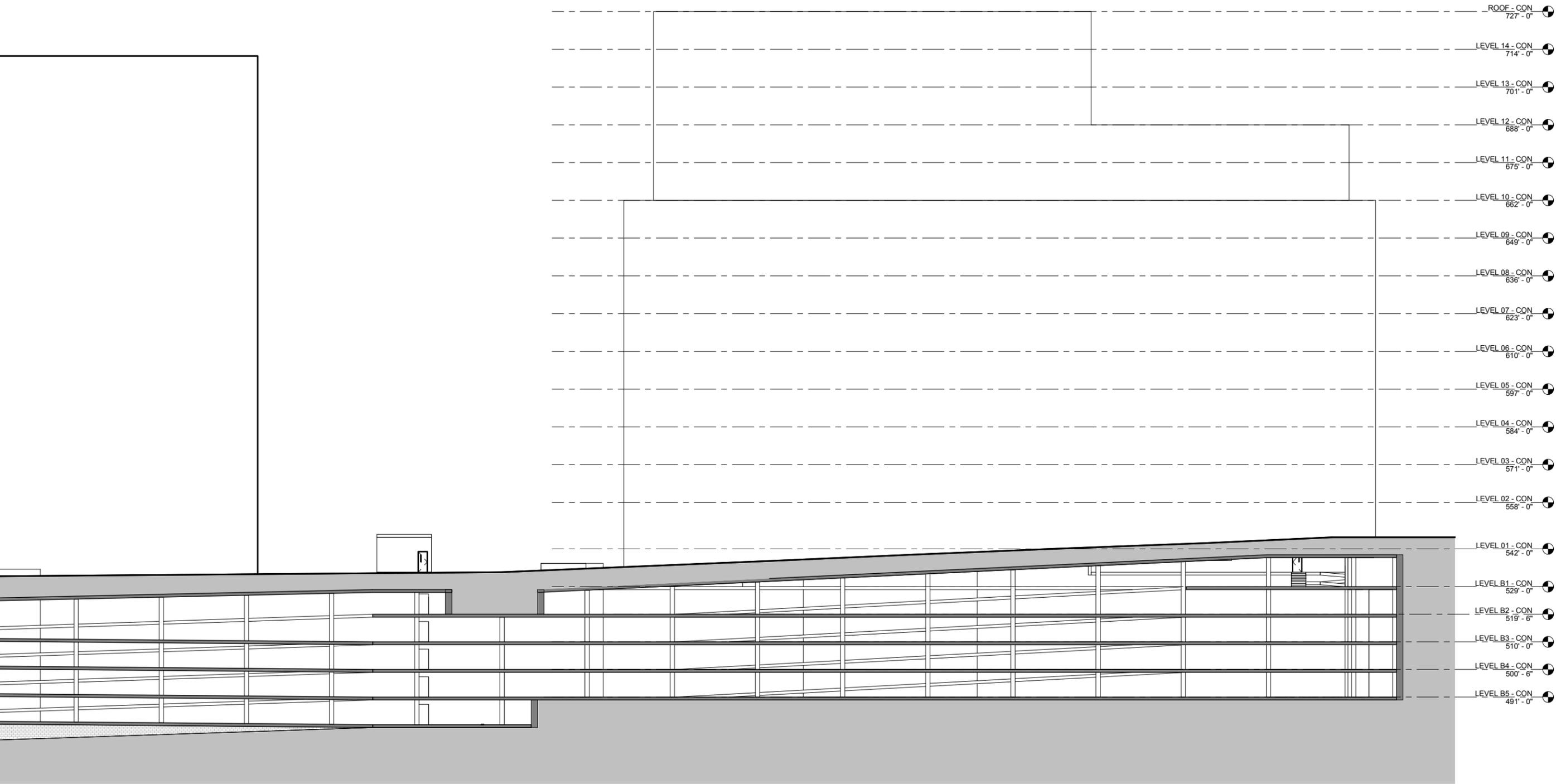


Figure 2.11
Longitudinal Mall Garage Section at Congress - Phase 1

Congress Building



William B Travis Building

William B Travis Building

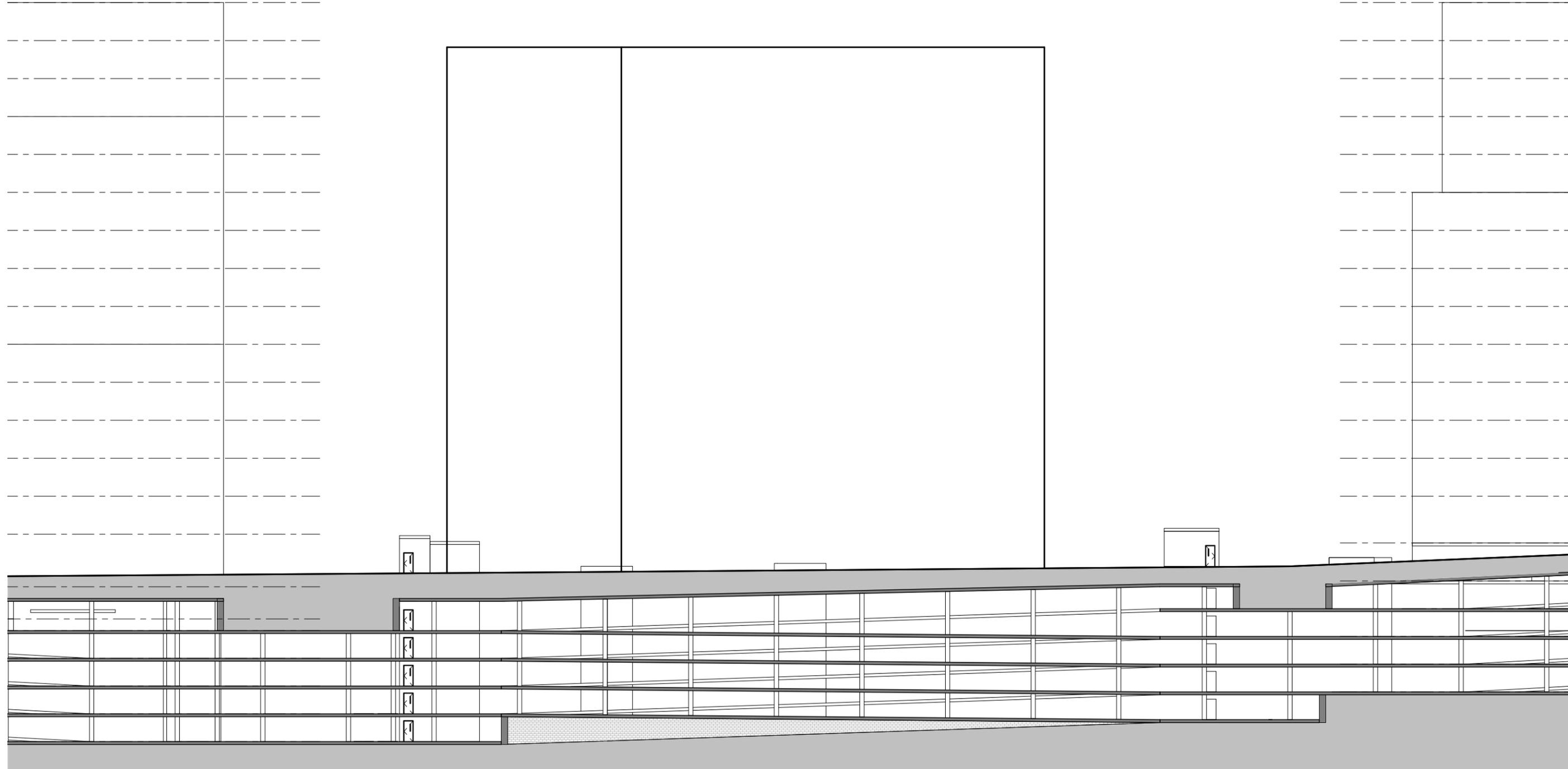
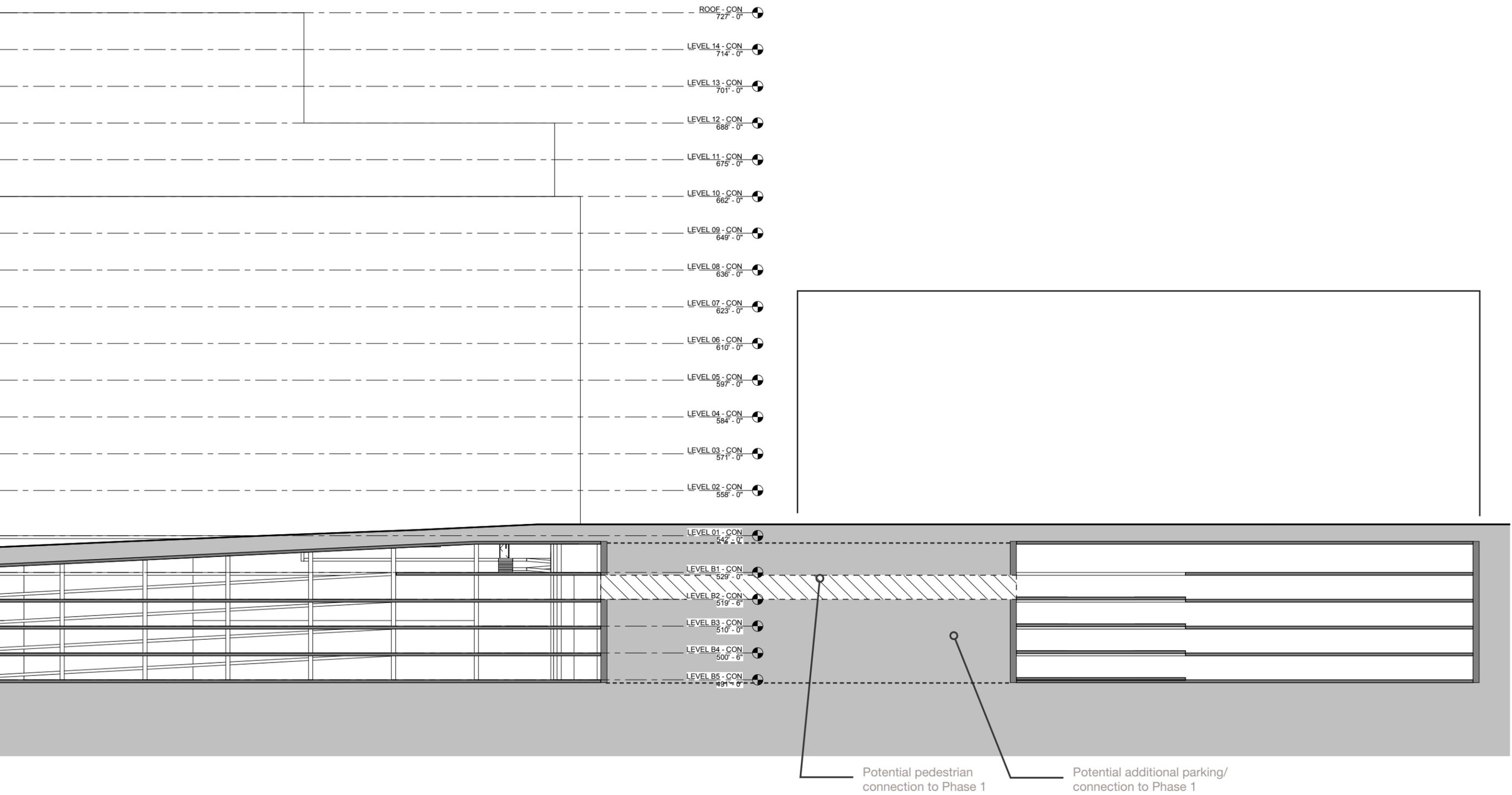


Figure 2.12
Longitudinal Mall Garage Section at Congress - Phase 1-2

Congress Building

Robert E Johnson Building



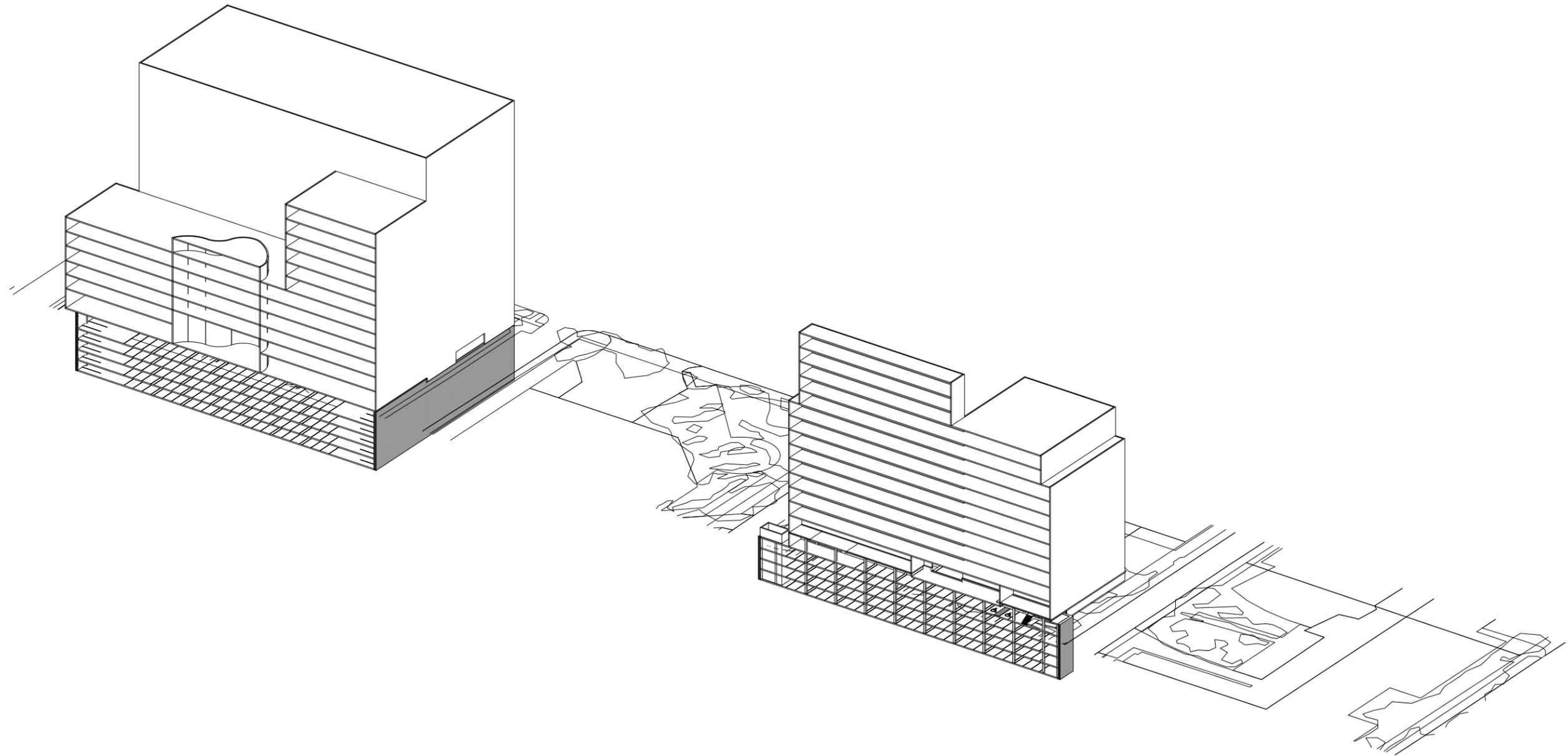


Figure 2.13
Axonometric Section - MLK Building & Congress Building - Phase 1

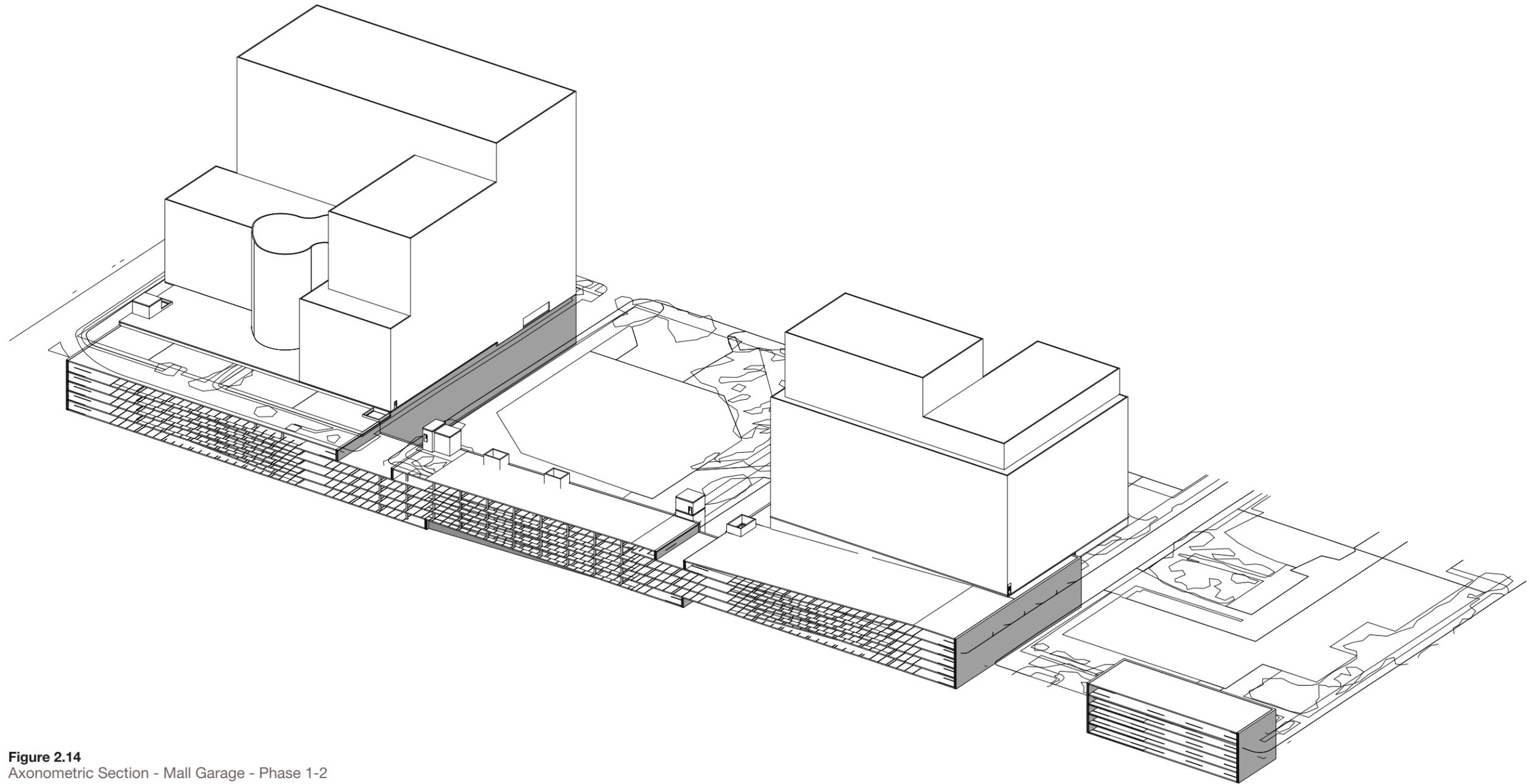


Figure 2.14
Axonometric Section - Mall Garage - Phase 1-2

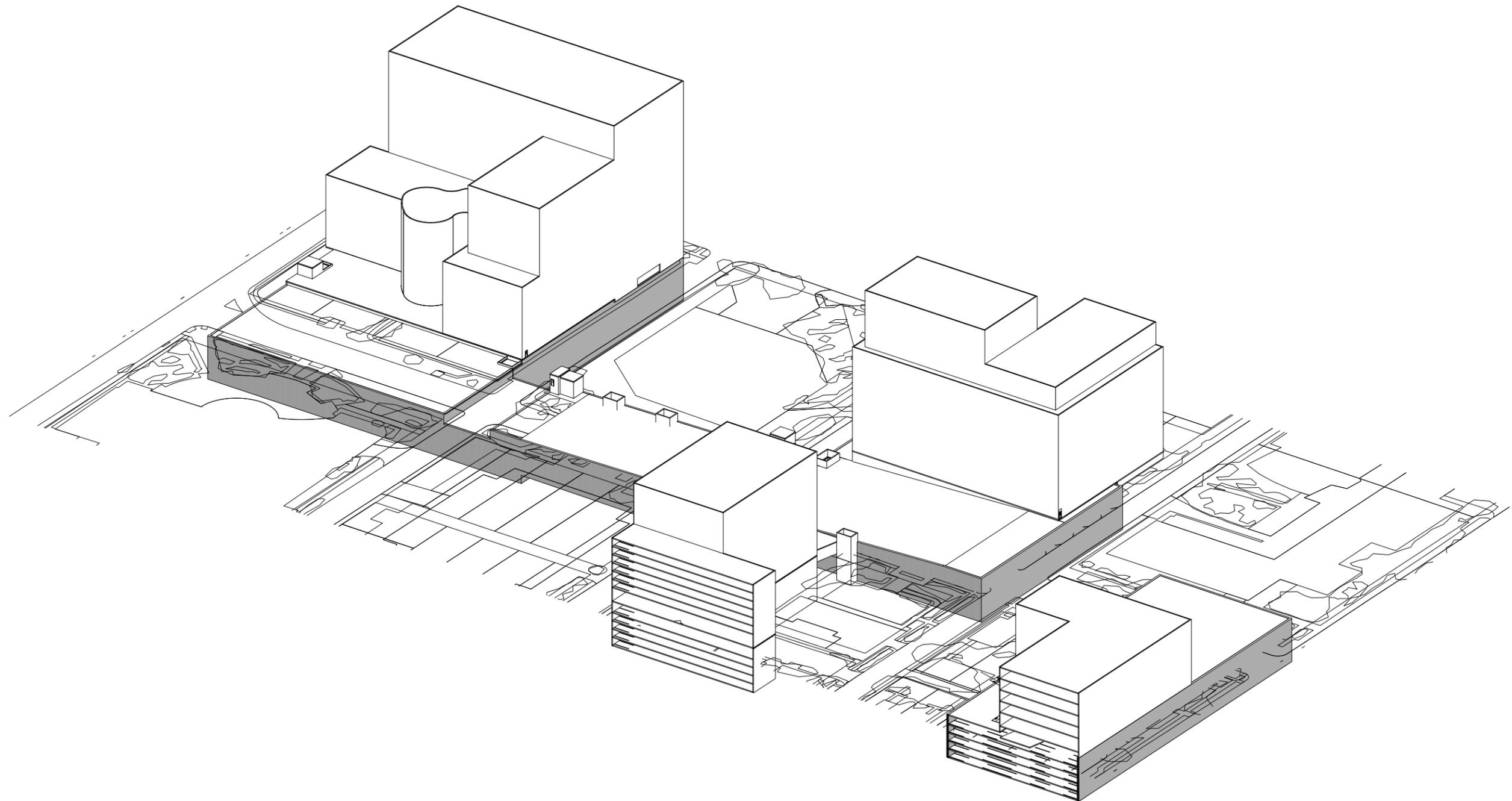


Figure 2.15
Axonometric Section - 17th Street Building & 15th Street Building - Phase 1-3

DESIGN ACCOMMODATION

The included drawings represent that, at a high-level, the master plan features can be accommodated as desired. The total building size and associated parking spaces are met through this conceptual design. However, certain challenges do exist that will need to be considered during the detailed design process.

Below is a summary of the primary challenges for each building site.

MLK site

- The garage side of the site must accommodate three separate vehicular access points for above- and below-grade parking and loading dock while maintaining required driveway spacing and distance from intersections.

Congress site

- Loading and garage access point share a driveway along 17th Street due to site constraints and required CCF outdoor space along 16th Street.
- The garage below the site and adjacent Mall panel will need to mitigate a significant site slope from southwest to northeast.
- The CCF requires that all classrooms have ground floor egress, which will require a detailed programming effort within the building's constrained urban site.

15th Street site

- Below-grade parking must consider proximity to Gethsemane Lutheran Church and the Robert E. Johnson Building basement, which extends farther west than the primary building facade

17th Street site

- Phase 1 will require a below-grade parking entry within this site; that entry must be incorporated, replaced, or reconstructed at the time the Phase 3 building is constructed.

Parking

As noted in the building requirements, each building has a target parking space accommodation. Though additional factors must be considered in the ultimate project design, the above parking layouts were studied with a concept for integration with egress stairs and ventilation shafts. Accessible parking spaces are shown for a conceptual approach but do not represent the calculated requirement of these spaces.

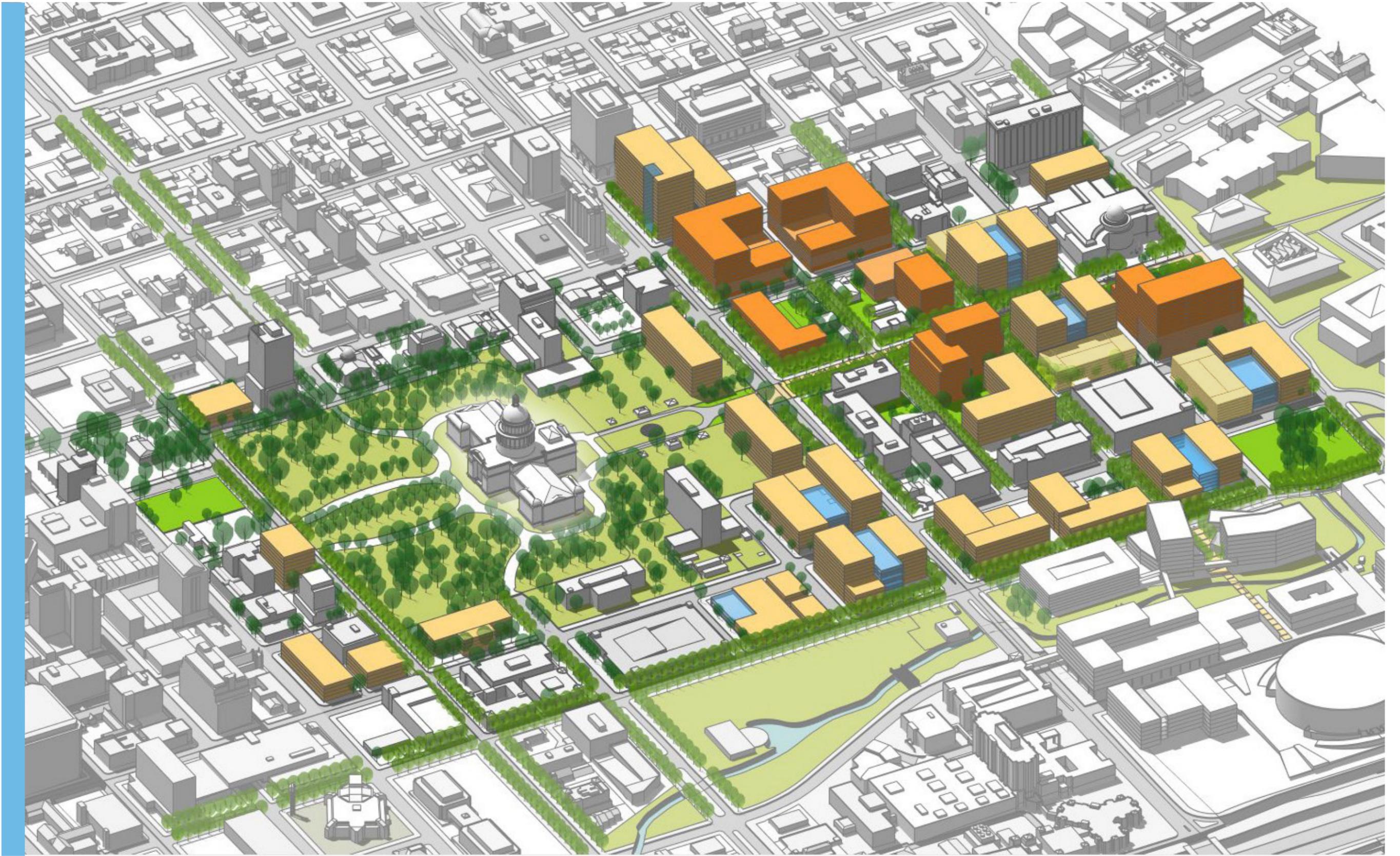
The following are the parking space accommodations shown in this study, all of which meet the state objects by phase.

Phase 1

- MLK site
 - Typical below-grade parking level = 350
 - Typical above-grade parking level = 97
- Congress site
 - Typical below-grade parking level = 189
 - Child Care Facility Drop-off Zone = 29
- Mall Connector
 - Typical below-grade parking level = 155
- Phase 1 total spaces = 3,846

Phase 2 & 3

- 15th Street site
 - Typical below-grade parking level = 221
 - Total spaces = 1,049
- 17th Street site
 - Typical below-grade parking level = 113
 - Typical above-grade parking level = 62
 - Total spaces = 921



A large, white, stylized number '3' is positioned on the left side of a solid blue rectangular area. The number is bold and has rounded edges.

Conceptual Texas Mall Drawings

The Texas Mall will establish a clear axis along what is currently Congress Avenue, aligned with the Capitol building. It will provide a continuous public landscape, connecting the fabric of the district and also a providing venue for events.

The purpose of this study is to determine the impact of the proposed underground garage infrastructure on the Mall design and to determine if any “fatal flaws” exist. It also investigates the impact the proposed Mall may have on adjacent properties¹.

The following elements will comprise the Texas Mall:

- 50-foot wide central lawn panel that is continuous between Martin Luther King, Jr. Boulevard (MLK) and 15th Street, with breaks at existing cross-streets to allow east/west traffic to cross
- 20-foot wide paved pedestrian way on either side of the central lawn panel, with the exception of the MLK to 18th Street block
- 8-foot wide sidewalk, 8-foot wide bus drop-off lane, and 12-foot wide bus travel lane on either side of the central lawn panel on the MLK to 18th Street block to serve the Texas History Museum and the future facility across Congress Avenue
- Staggered, double row of Live Oak trees located behind the paved pedestrian way in an area that provides for a transition to the existing or proposed buildings
- Access stairs and mechanical air vents for the garage integrated into the Mall

ALIGNMENT

The Mall will reinforce the strong visual axis along Congress Avenue to the grand Capitol building. In addition, it will create a large communal open space and pedestrian way connecting the east and west sides of the Mall. Therefore, the alignment is shaped by several factors:

- An axis along Congress Avenue between the Capitol building and MLK Boulevard
- General reflection of the current and historic alignment of Congress Avenue
- Minimized negative impacts on adjacent properties

¹ Impacts are determined based on the best site information currently available. The accuracy of this information is unconfirmed without having a site survey to reference for this study.

RELATIONSHIP TO GARAGE INFRASTRUCTURE

Due to its placement over the proposed garage, the Mall must accommodate the infrastructural elements necessary to move people and air between the Mall surface and the underground garage structure. Vents and stair access towers will be required at certain locations in the Mall. These elements can be located where they will not obstruct the sightlines along the Mall and can sometimes be integrated into other improvements. Stair towers should respect the architectural character of existing site improvements and be as transparent as possible.

PRINCIPLES FOR INTERFACING WITH EXISTING PROPERTIES

The following principles guide the overall Mall design, particularly with respect to interfacing with existing adjacent properties. For guidelines that are location-specific, see individual block plans.

Trees

Wherever feasible, existing trees will be integrated into the staggered, double row pattern to maintain the maximum number of healthy existing trees, while balancing the desire for a consistent, continuous canopy.

In locations where the existing conditions do not allow for a continuous double row of trees, continue the pattern as soon as possible beyond the conflict area.

Overlap with Existing Properties

Where the Mall improvements overlap with existing property improvements, some building frontage areas will require site reconstruction. Integrating the proposed Mall with existing plazas, planting areas, lawns, and sidewalks will require sensitivity and site-specific attention to create a functional and legible transition. In several cases, an existing sidewalk can be removed entirely, eliminating redundancy with the Mall pedestrian way.

Stormwater

The lawn panels will be slightly crowned and the paved pedestrian ways will be graded to drain away from the centerline of the Mall. In some cases, this configuration will require stormwater infrastructure adjustments within the transition area between the existing adjacent grade and the Mall. No areas have been identified where the interface between the proposed Mall and the existing grades are problematic.

In locations where new buildings are proposed, the grade of the Mall will dictate the finished floor elevation of proposed buildings and the stormwater strategies in proposed plazas and connecting landscapes.

Garage Access Stairs

Proposed garage access stair locations will serve the access needs within the garage, balanced with the context above ground. They will be located to minimize physical and visual impacts on the Mall, allowing for an unobstructed view and travel path along the Mall as well as sensitively integrating with existing buildings and landscapes. In some cases, access stairs can be located within proposed buildings to reduce the number of structures penetrating the ground surface around the Mall. This is will occur wherever possible.

Access stairs should be designed to be as transparent and unobtrusive as possible to allow the Mall and Capitol to hold the visual focus of the corridor.

Garage Vents

Exhaust vents will be located within proposed buildings wherever possible. The 18th to 17th Street block does not include new buildings, and therefore, will require that an exhaust vent is located near the Mall. It will be integrated with a stair tower to reduce the number of required structures.

Intake vents are flush with grade and located outside the Mall, often within planting areas to reduce their visual impact.



Figure 3.1
Mall Conceptual Layout



MLK to 18th Street Block

Texas State History Museum Plaza

The plaza in front of the Texas State History Museum is a beloved landscape and is important to the ongoing operations of the museum.

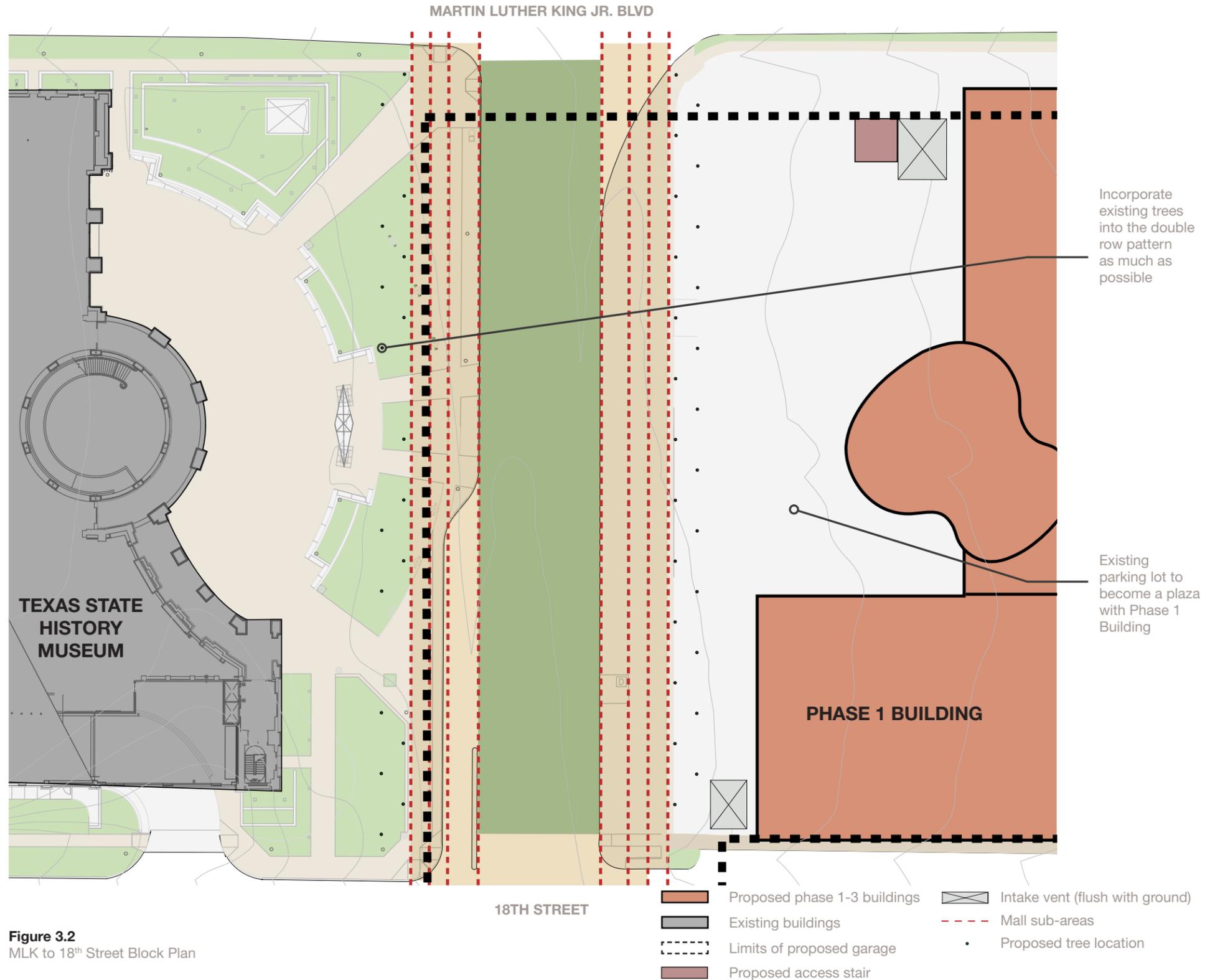
Impact on the Texas star sculpture is of particular concern. According to the best site information currently available, the star likely will not require relocation or removal. However, several trees may need to be removed where the garage excavation or proposed paved areas occur. Any existing trees that can be integrated into the double row pattern will remain.

The western edge of the Texas State History Museum block has significant overlap with the proposed Mall and will require a careful re-design to create an appropriate interface that should be mutually beneficial to the museum and the Mall visitors.

Phase 1 Building Plaza at MLK

Along with the construction of the Phase 1 Building on the MLK site, a new plaza will replace the existing parking lot. Since the entire block is being re-designed, there is a great opportunity for the proposed Mall design to be integrated into the new plaza.

Vents and stairs will be located as close to the proposed building as possible to avoid significant impact on the museum across the street, the proposed plaza, or the Mall itself.



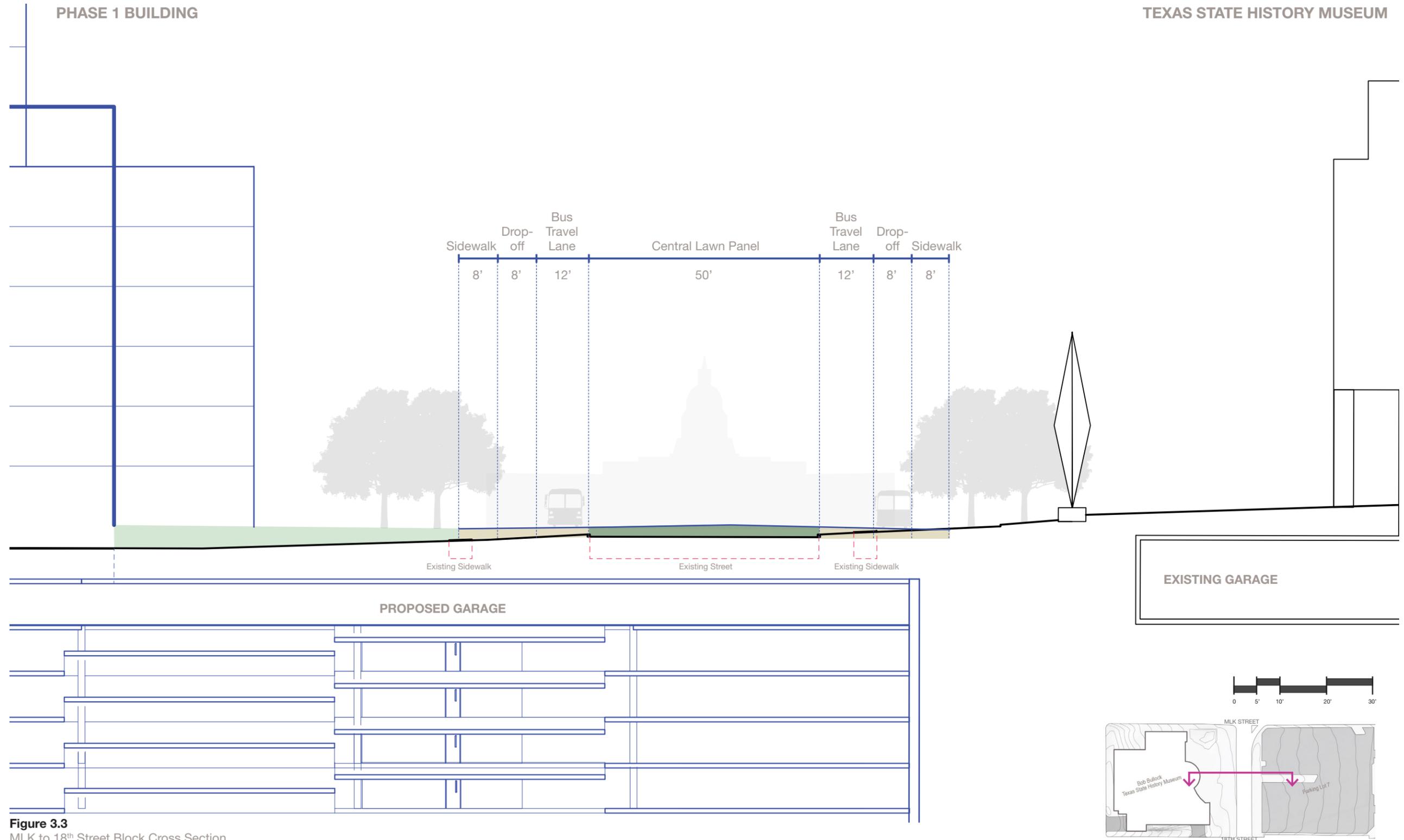


Figure 3.3
MLK to 18th Street Block Cross Section

18th to 17th Street Block

Stephen F. Austin Frontage

According to the best base information currently available, it appears that the Stephen F. Austin steps and plaza will not be impacted. However, the tree planters on the street side of the steps will need to either be reduced in size or removed entirely. The double row of trees may require a wider planting area, necessitating the removal of the existing sidewalk.

William B. Travis Frontage

The William B. Travis frontage will accommodate several components of the garage infrastructure, including two access stair towers. The tower at the north side of the block can be combined with an exhaust vent and located outside of the Mall improvement area. Removal of the existing sidewalk will allow the existing raised planting bed to remain and provide a minimally visually invasive location for the combined structure.

The access stair tower on the southern end of the block will be located to minimize impact on both the Mall and the entrance plaza to the William B. Travis building. Impact from the garage construction will necessitate a re-design of this entry plaza, which will provide an opportunity to better integrate with the proposed Mall.

Intake vents can be located between the existing sidewalk location and the wall. The existing sidewalk can be removed and become a planting area, thereby visually obscuring the vents.

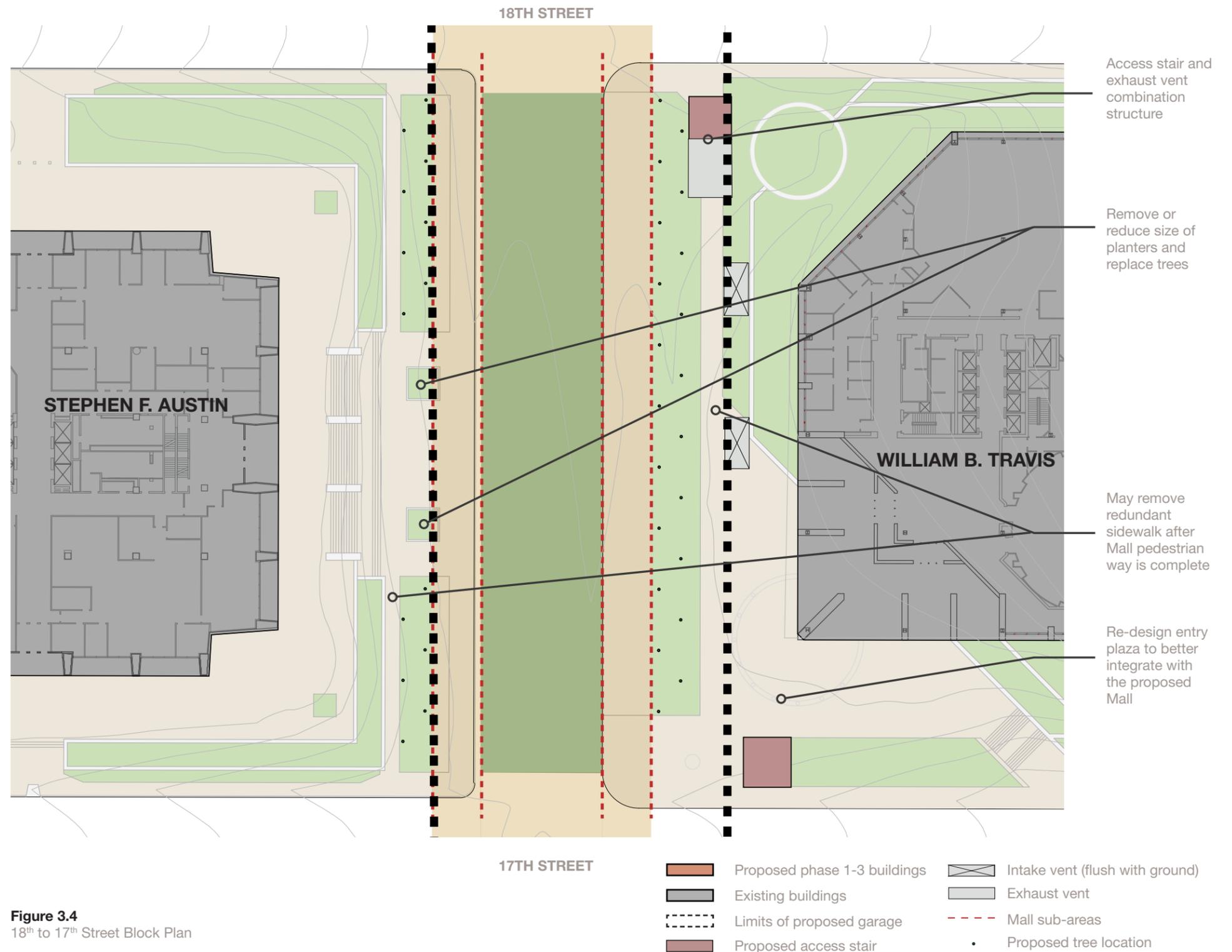


Figure 3.4
18th to 17th Street Block Plan

WILLIAM B. TRAVIS

STEPHEN F. AUSTIN

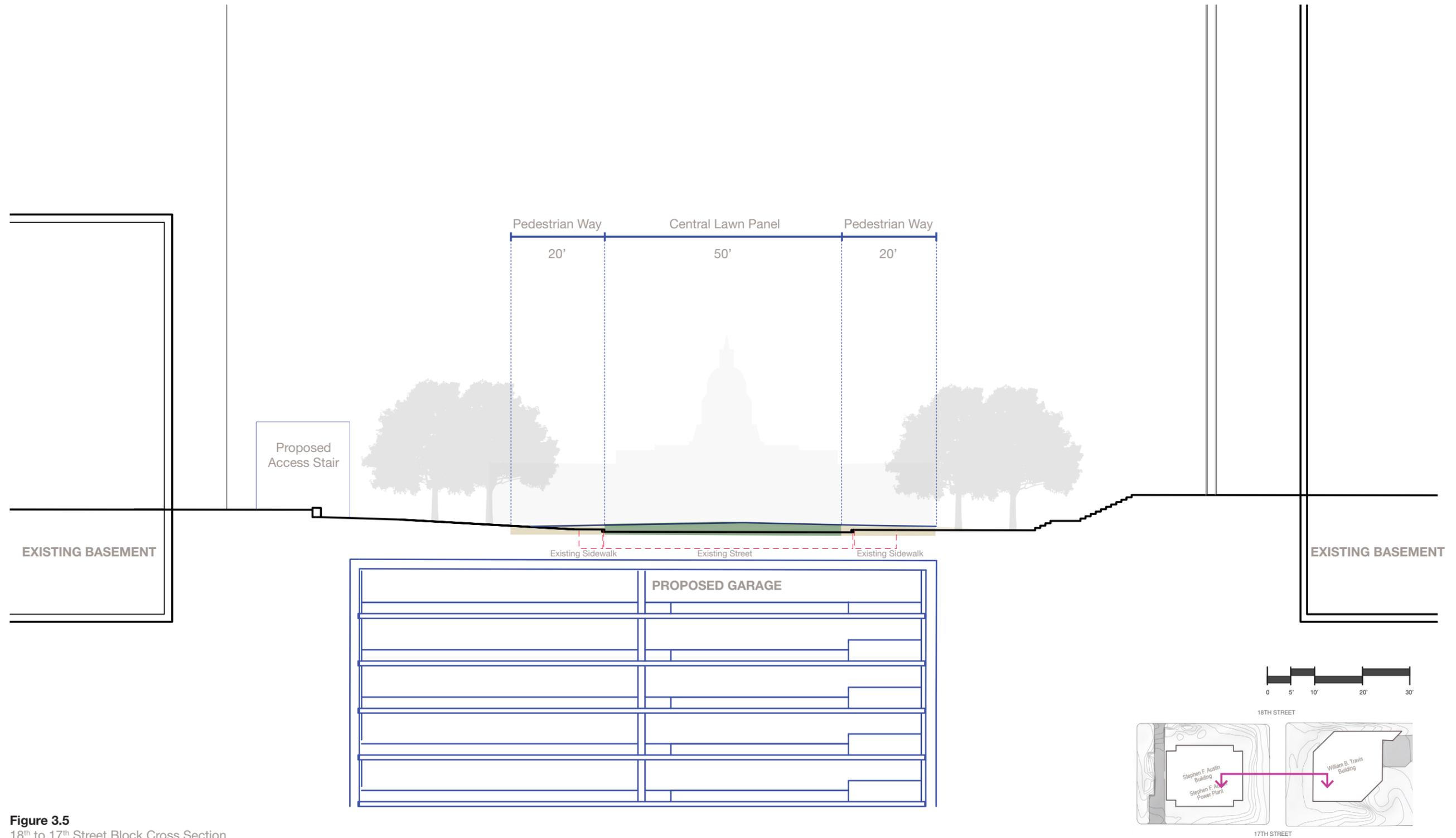


Figure 3.5
18th to 17th Street Block Cross Section

17th to 16th Street Block

Phase 3 Building Plaza

In conjunction with the construction of the Phase 3 Building, a new plaza will replace the existing parking lot, which will provide an opportunity for the future plaza to be integrated with the proposed Mall design.

A proposed garage entry will ramp down from 17th Street beneath the proposed Phase 3 plaza to access the garage underground. Adequate soil depth will be provided to support the double row of trees.

Intake vents at grade will be located between the two rows of trees to reduce their visibility.

Diocese Frontage

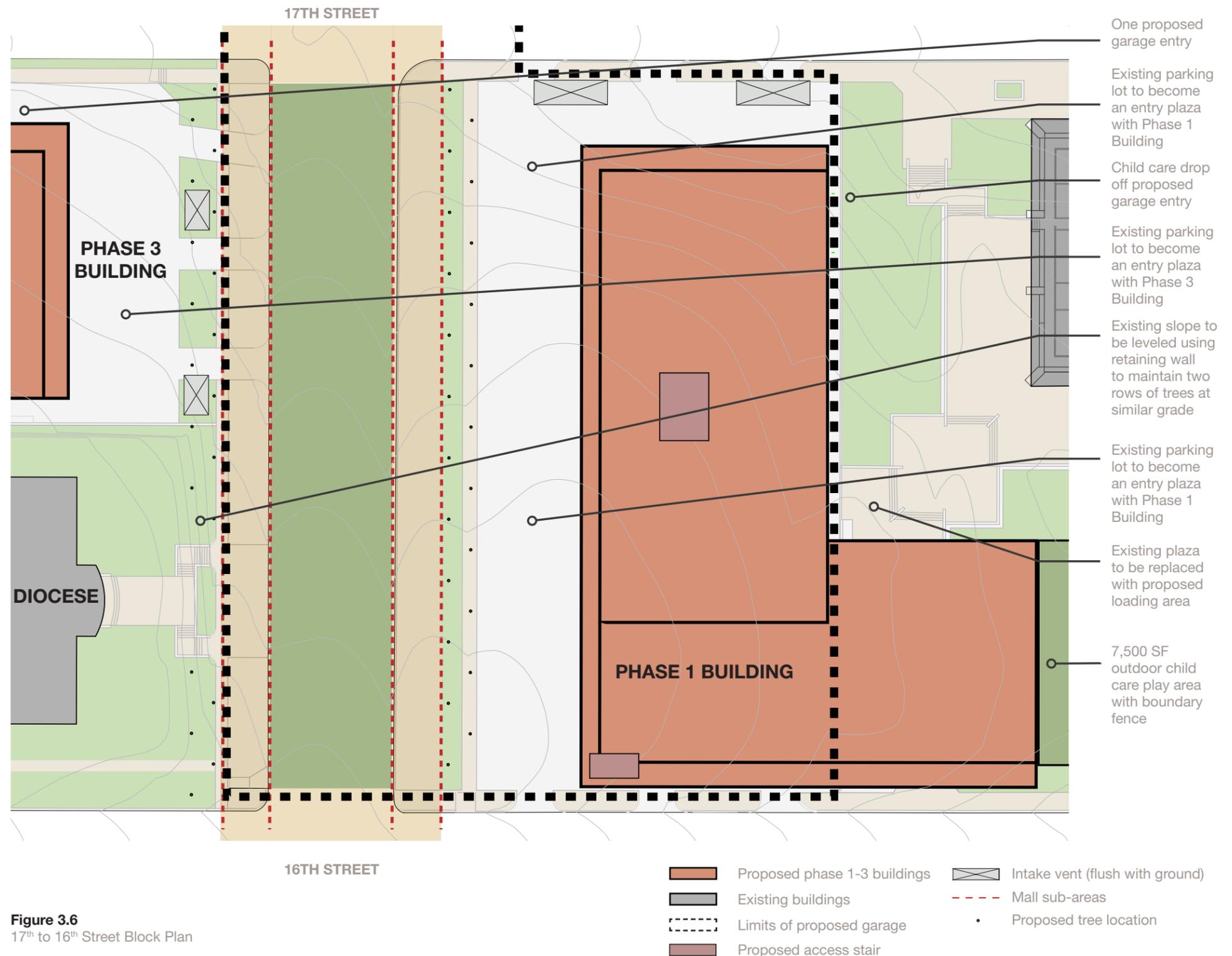
The Diocese property has a significant slope down from the building to the street, requiring steps and retaining walls to access street level. According to the best information currently available, the Mall improvements are not likely to impact these steps and walls. Additional retaining walls on either side of the steps will create a flat area adjacent to the Mall and allow continuation of the planted row of trees at a consistent grade.

Phase 1 Building Area

The Phase 1 Building and a new plaza will replace the existing parking lot, which will provide an opportunity for the proposed Mall design to be integrated with the future plaza. Exhaust vents will be located within the building, and intake vents will occur behind the sidewalk, at least 10 feet from the street.

A garage entry for the child care drop-off and pick-up will enter under the Phase 1 Building.

7,500 square feet of outdoor fenced area will serve as play area for the child care facility east of the building. The play area is of adequate size but additional study will be required to determine the layout.



PHASE 1 BUILDING

CATHOLIC DIOCESE CHANCERY

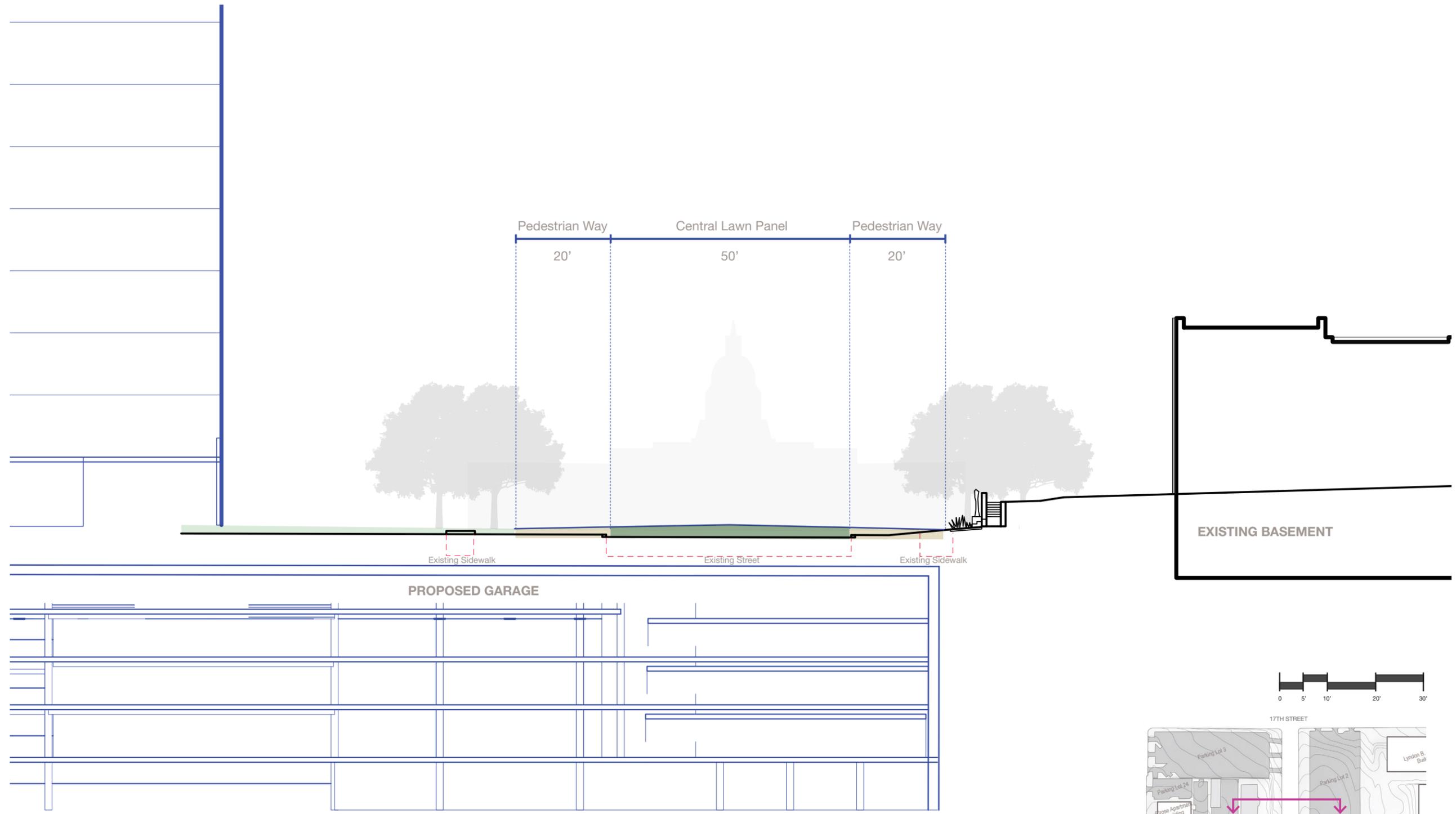


Figure 3.7
17th to 16th Street Block Cross Section

3 16th to 15th Street Block

Gethsemane Lutheran Church

The church property has a significant slope down from the building to the street, requiring steps and retaining walls to access street level. According to the best information currently available, the Mall improvements are not likely to impact these steps and walls. Additional retaining walls on either side of the steps will create a flat area adjacent to the Mall and allow continuation of the planted row of trees at a consistent grade.

Phase 2 Building and Garage

A new building and plaza will replace the existing parking lot as a part of Phase 2 construction, which will provide an opportunity for the future plaza to be integrated with proposed Mall design.

Since design of this garage has not yet been developed, intake vents may change in size and position from where currently shown. The full impact on the existing Robert E. Johnson frontage landscape is also unclear, but the building itself will not be impacted. Depending upon the ultimate footprint of the Phase 2 garage, the landscape adjacent to the Mall will likely require replacement following the excavation. The surrounding plazas and planting areas will also require re-design to better integrate with the proposed Mall.

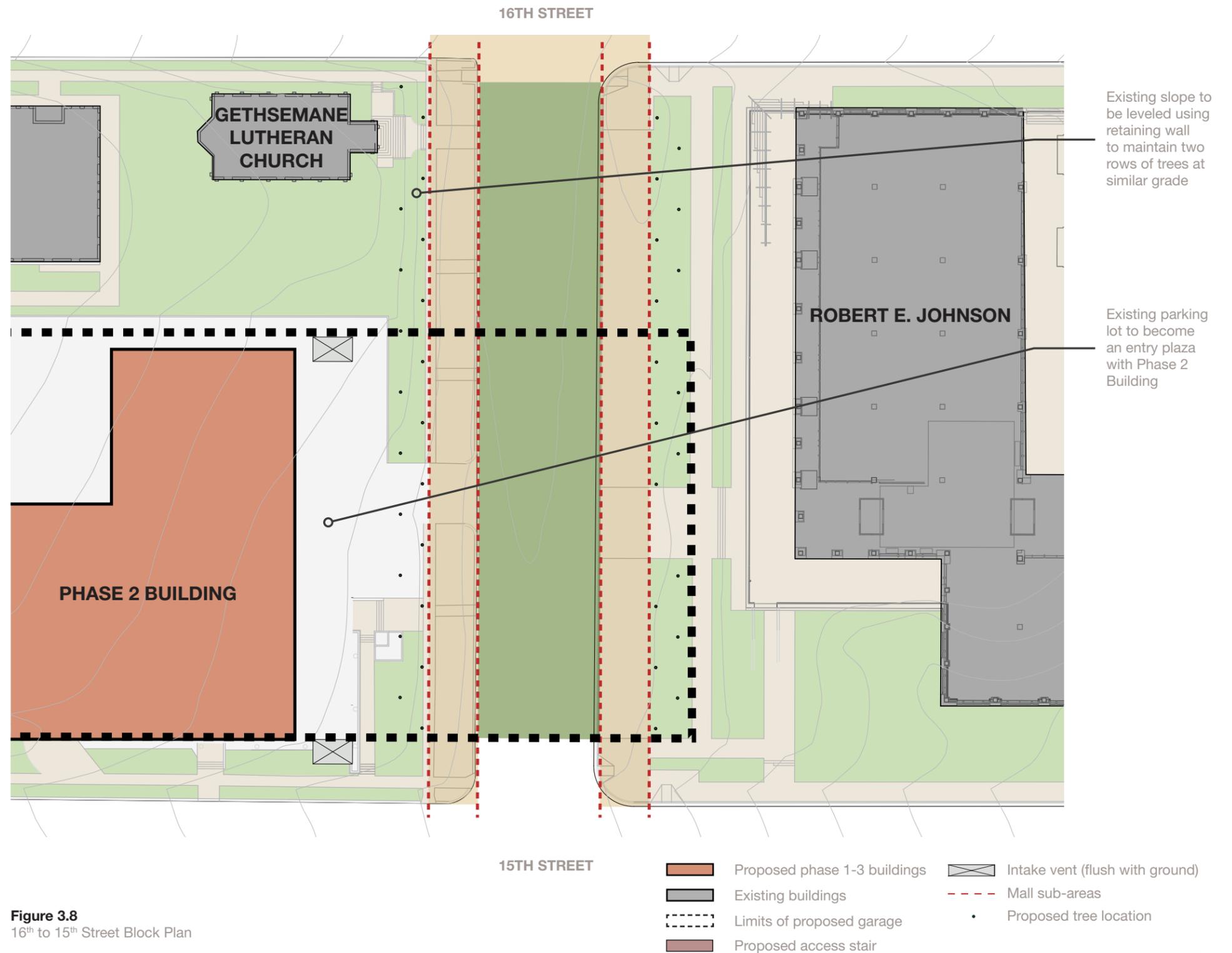


Figure 3.8
16th to 15th Street Block Plan

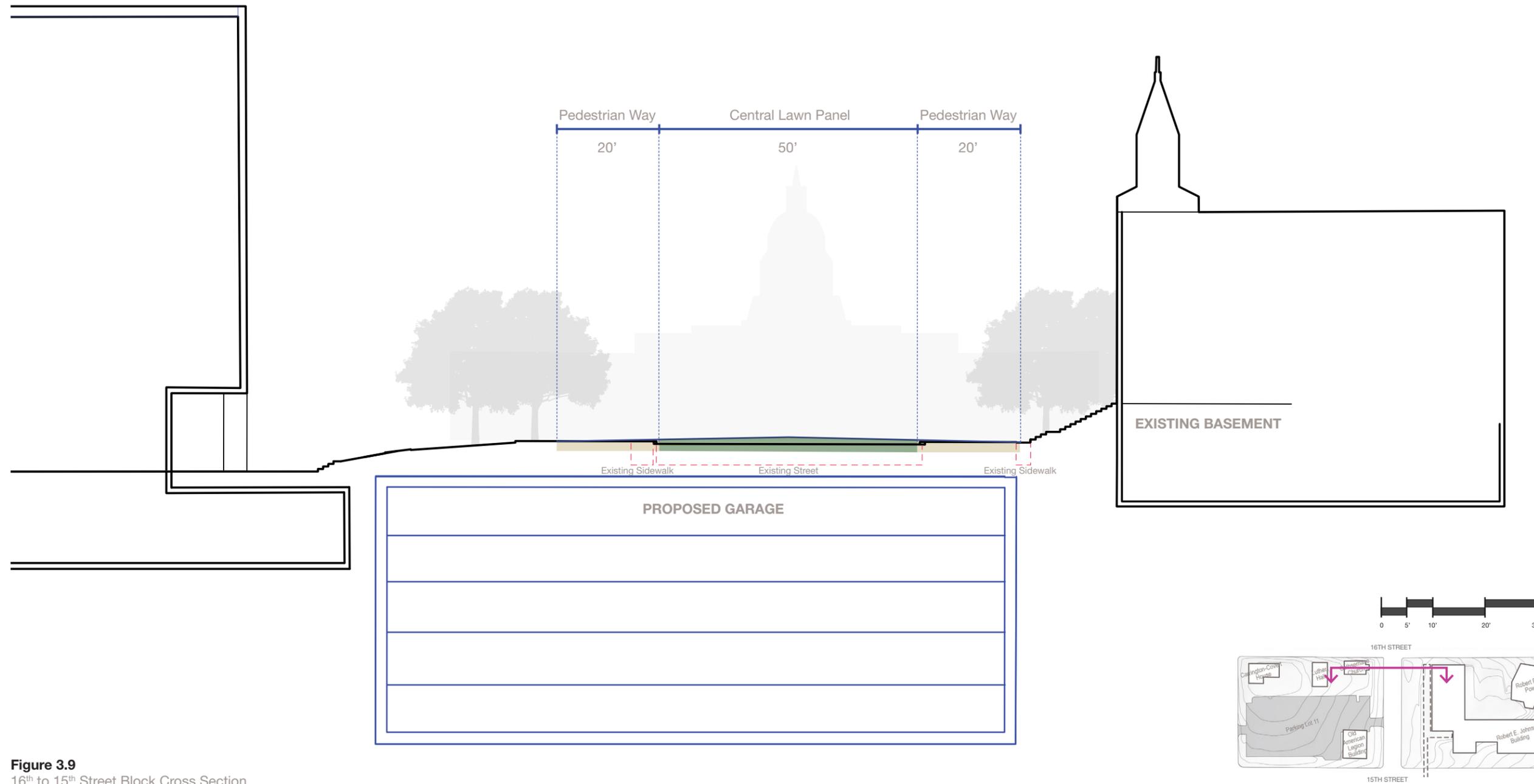


Figure 3.9
16th to 15th Street Block Cross Section



4

City of Austin Coordination

4 City of Austin Coordination

A series of meetings have been held with City of Austin staff to begin the process of coordination between the City and the State for development within the Capitol Complex. Though State property does not fall within the City of Austin jurisdiction, much of the work that is proposed in Phases 1 through 3 will occur within the public right-of-way, which falls under City jurisdiction. Most significantly, the new Texas Mall will require the vacation of a current street in order to convert it to a pedestrian environment. The Texas Facilities Commission is committed to working with the City of Austin to ensure the process goes smoothly and results in the best outcome for the State, while also providing benefit to the surrounding community.

During the high-level meetings, leadership from multiple City departments weighed in on specific processes, impacts, and points of coordination that will be triggered by the development within the Capitol Complex. The following is a summary of the primary discussion points. All will require further investigation during a detailed design process.

UTILITIES

- The vacation of Congress Avenue may require a new easement for utilities.
- Austin Energy would prefer to run services along the Texas Mall with access points; they would be open to running conduit through the mall garage if necessary, though there is a concern for public safety with an exposed conduit in their 35 kV network.
- Drainage for the development outflows into Waller Creek; conveyance should be considered versus detention.
- It will be the responsibility of the detailed design team to determine a proposal for the rerouting of utilities in the area to be occupied by the Texas Mall and associated parking structure.

TRANSPORTATION

- The aesthetics along 15th Street are very important to the City.
- The master plan traffic impact analysis should investigate if 6 lanes are truly needed along 15th Street, as the nearby hospital construction will reduce that portion of 15th Street to two lanes.
- The mall garage parking should investigate the access points and consider working with the City on technology utilized to find parking.
- One-way to two-way conversion of 16th, 17th, and 18th Streets is likely acceptable to the City; signals and striping are the primary work this would require; the traffic study should consider conversion outside the boundaries of the Complex to match up to the points where these streets already transition to two-way traffic.
- The Mall as a dismount zone for bicycles will be a difficult policy to enforce.
- The construction process should consider workforce parking at a remote parking facility to reduce parking demand and traffic around the construction site.

MALL USE

- There is interest in what types of special events will be allowed to utilize the Mall once complete.
- TFC should begin coordinating with the entities that currently utilize Congress Avenue for events and notify them of when it will no longer be available due to the construction process.

FIRE AND LIFE SAFETY

- Austin Fire Department would like to work with the State Fire Marshal during the design process, since the buildings will be under the State Fire Marshal jurisdiction but Austin Fire Department will respond to any emergencies.
- The offset from the building face to the emergency path along the wall should be considered; the aerial reach is 105'.
- The design should ensure that trees and furnishings are not within the emergency path to allow clear access for emergency vehicles.
- The underground garage should be structured to ensure that several fire department apparatus can be on the Mall emergency path at one time.
- The pedestrian pop-ups along the Mall from the garage can incorporate a Knox box as required for fire department access.
- The garage should have clear markings so that a person making an emergency call can easily identify his location within the garage; compartmentalization of the garage can narrow the scope of where an emergency call is made from.

Egress Accommodation

Any future design will need to meet all applicable building codes. One primary plan driver for the below-grade parking garage will be the location of egress point. No point within the garage facility can be farther than 200' from an egress, which in this case are stairs with a rated enclosure.

The diagram to the right indicates the conceptual egress stair locations for the Phase 1 below-grade parking garage. Each stair is overlaid with a circle of 200' radius. This study took a basic view of requiring every point within the garage to be contained within one of these circles. Because the top level of the garage is discontinuous, it is assumed that occupants cannot traverse between the three portions of the garage from an egress standpoint. Further detailed study will be required during design to determine if other obstacles are in place that may impact the exit separation.

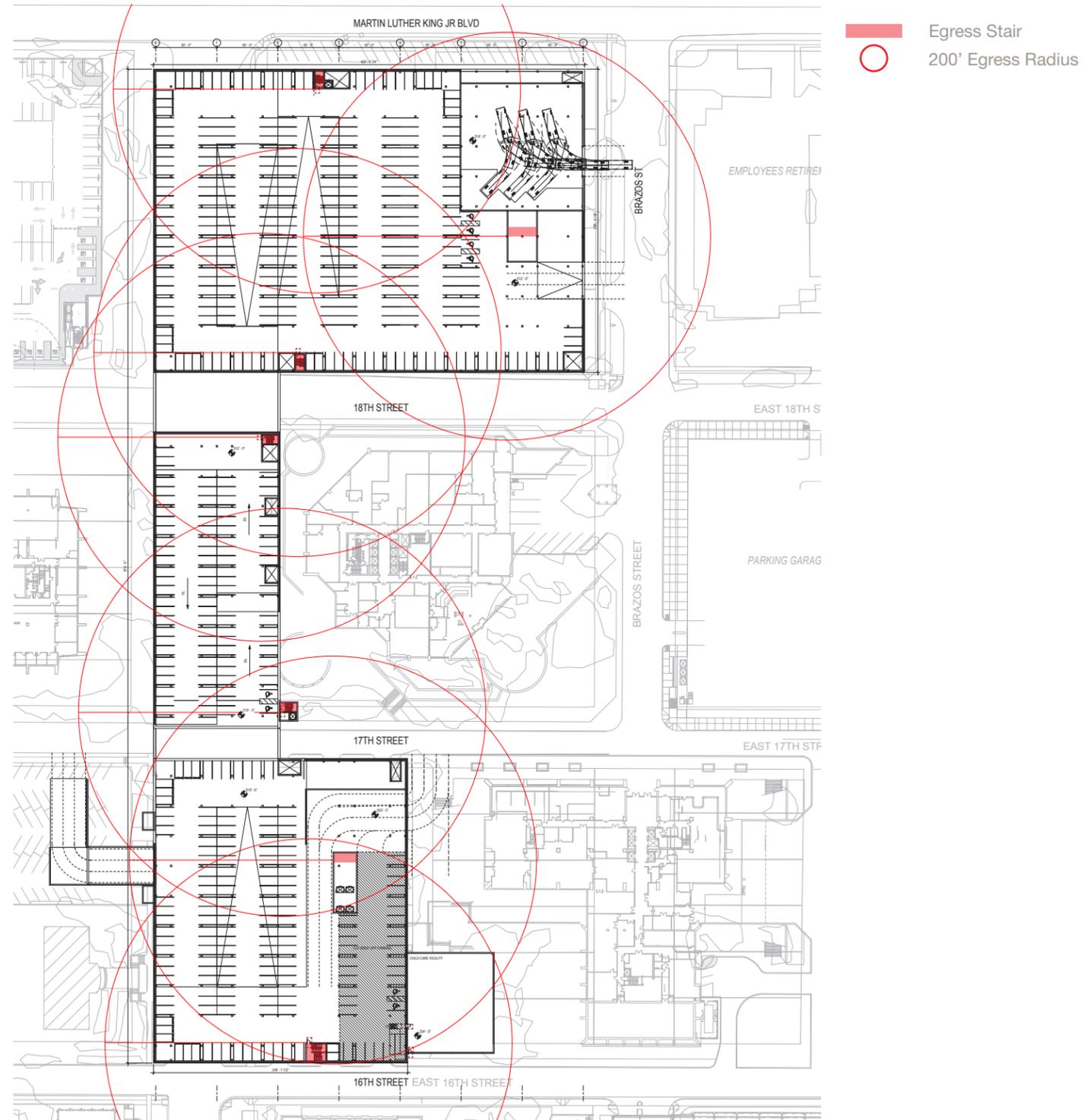


Figure 4.1
Conceptual Parking Garage Egress Layout - Phase 1



5

Security / Child Care

Security / Child Care

The Phase 1 development of the master plan will primarily provide office space for state agencies currently occupying commercial lease space throughout Austin. However, two additional entities currently located in the Capitol Complex will also be relocated to the Phase 1 buildings. These are the Department of Public Safety Region VII Headquarters and the Child Care Facility. The planning team met with each group to obtain a high-level summary of their needs and requirements.

SECURITY

The master plan proposes a temporary move of the Department of Public Safety (DPS) as part of Phase 1 development. This move allows the existing DPS building at the corner of 15th Street and Congress Avenue to be vacated, making space for Phase 2 development at the 15th Street site. The 15th Street site is considered to be the permanent location for DPS following a temporary move in Phase 1.

A meeting was held with DPS to determine the security needs of the new Texas Mall, facilities, and the extensive below-grade parking structure. General assumptions of requirements were provided, though a detailed programming effort will be necessary prior to detailed design.

Security Requirements

The following represents an overview of the initial security suggestions DPS provided for the new Texas Mall and associated parking garage:

- The highest threat areas of the Texas Mall should incorporate monitored cameras.
- The parking garage should incorporate separation between State employee and public parking.
- It is desired that sections of the parking garage can be closed after business hours, except for special events.
- Vehicular queuing is not preferred inside the garage
- Public parking should not be located below areas of high importance, such as the Child Care Facility.
- Garage access points from street level will need to incorporate restrictions to specific areas of the lower garage.

DPS Facility Needs

The Phase 1 relocation of the DPS office is considered to be a temporary move while the existing building site is cleared for Phase 2 development. The Phase 2 15th Street building site provides an optimal permanent location for DPS due to its proximity to the Capitol and direct access to a major street.

The temporary Phase 1 move is preferred to be at the MLK site so that it is not located in the same building as the Child Care Facility. DPS prefers to maintain a small presence in the MLK building after permanent relocation in Phase 2. This can be approximately 1000 SF at the ground level and will provide better coverage of the entire Texas Mall.

The permanent Phase 2 facility for DPS should consider the following programmatic accommodations:

- Trailer staging for horses
- Short-term animal storage
- Helipad
- Emergency management team area
- Connection to the existing underground tunnels
- Garage separation between DPS vehicle parking and other state employee parking

The Phase 2 15th Street building will need to have a higher level of security to house the DPS offices. Since DPS will not occupy the entire building, other suitable state agencies should be considered that will be compatible with the required level of security. One such example agency could be the Attorney General's Office.

CHILD CARE

The master plan proposes a new facility to be placed in the Phase 1 Congress building. This allows the existing facility to be vacated, making space for Phase 3 development at the Lavaca site. A meeting was held with the Child Care Advocacy Committee to make high-level assumptions of the needs and requirements of a relocated Child Care Facility within the Capitol Complex. All of the provided assumptions will require a more detailed programming effort to determine the final space needs.

The following represents a high-level list of potential requirements for a new Child Care Facility:

- Facility capacity of 200 children
- 35 NSF/child for classroom space
- Each classroom must have a ground floor exit
 - Facility can be multiple levels if this requirement is met
 - Children under 2 years old cannot be on upper floors
- Drop-off/pick-up parking to provide 30 spaces
- 80 SF/child of outdoor spaces
 - Assume 50 children at a time in older children's area
 - Assume 35 children at a time in toddler area
 - Additional small infant outdoor area to be provided
- Outdoor fencing is required to be 4 feet high
 - 6 feet height is preferred for fencing
 - Fencing is to be transparent

Space Accommodation

In total, the above requirements result in approximately 15,000 GSF of interior space and 7,500 GSF of exterior outdoor play area. The basic design included in this document meets the required exterior area with no extra space. The ground floor footprint shown represents less than the required area. To accommodate the remaining area, a second story or mezzanine could be added. Alternatively, the facility could extend farther west on the first floor of the building, fronting on the Texas Mall at the west facade. In either case, the drawings indicate a multi-level or split-level condition. The design team will be required to coordinate with the State Fire Marshal to ensure that ground floor egress requirements are met for the entire facility.

Drop-off Parking

As noted above, the Child Care Facility requires 30 parking spaces for drop-off and pick-up of children. These spaces are to be located adjacent to the facility's entrance to provide easy access for parents.

The proposed location for the facility is on the first level of the Congress building, along 16th Street. This location has the potential for direct access to a secured outdoor area on the east side of the building. However, the urban setting of the building does not allow space for a surface parking lot. Consequently, drop-off parking is suggested on the upper level of the adjacent parking garage.

The plan and section diagrams to the right indicate a conceptual layout that utilizes the site's natural topography to provide parking access in the garage without traversing multiple stair levels or requiring an elevator. If the main entrance to the facility is located at grade along 16th Street, the primary level will be approximately 5 feet above the top level of the garage, which is accessed from 17th Street. This elevation change can be accommodated in the garage drop-off area through a small stair and associated ramp.

The location of the drop-off parking spaces within the garage allows for access just before the control point of the garage, meaning that parents would not be required to provide card access or other means of entering the garage to complete drop-off. The suggested main pedestrian entrance location at grade along 16th Street can also allow for some parallel parking spaces on the street for quick access without entering the garage. However, parallel parking spaces cannot reasonably provide the entirety of the drop-off parking needs, as this would require over 600 feet of length along the street frontage.

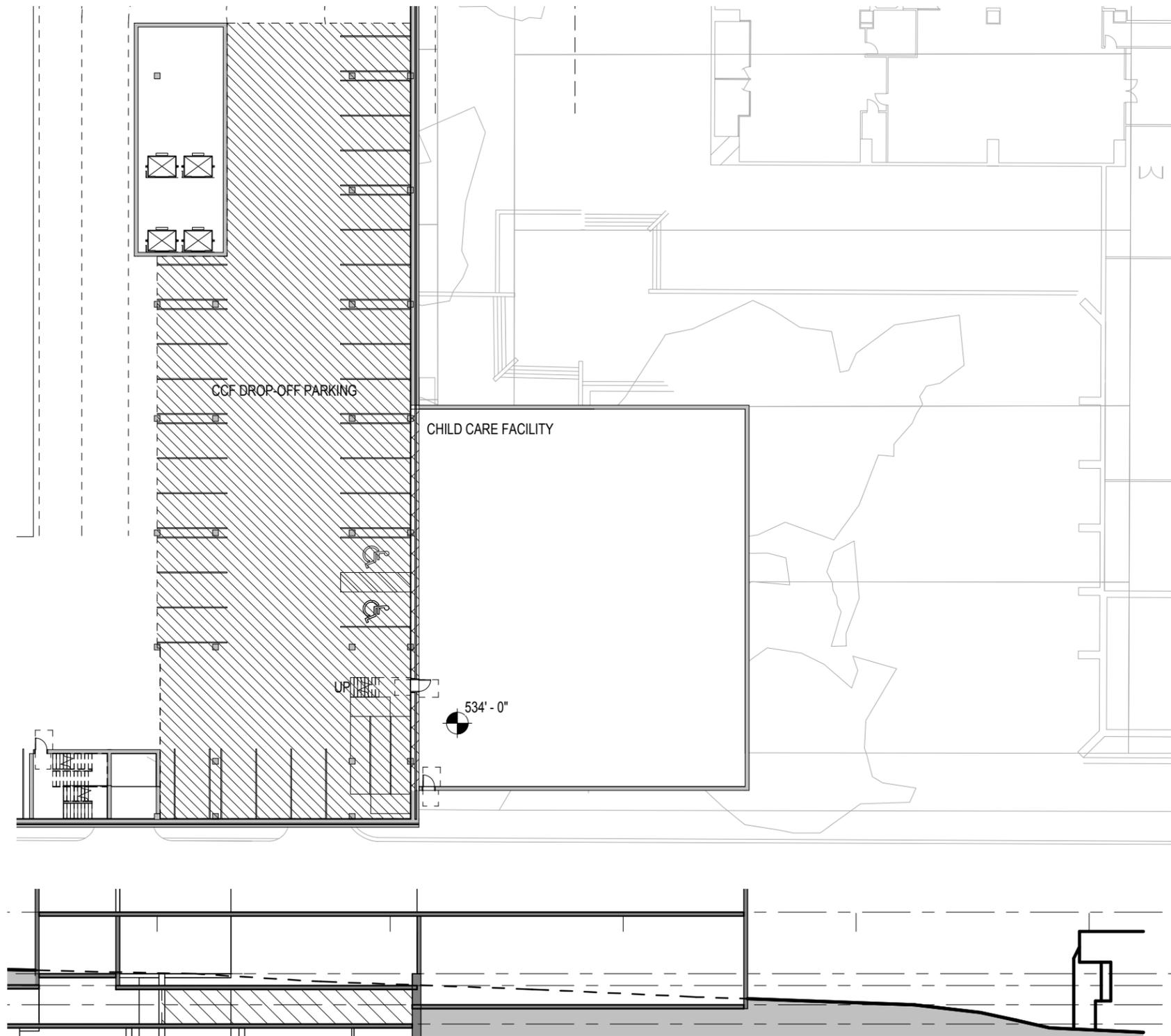
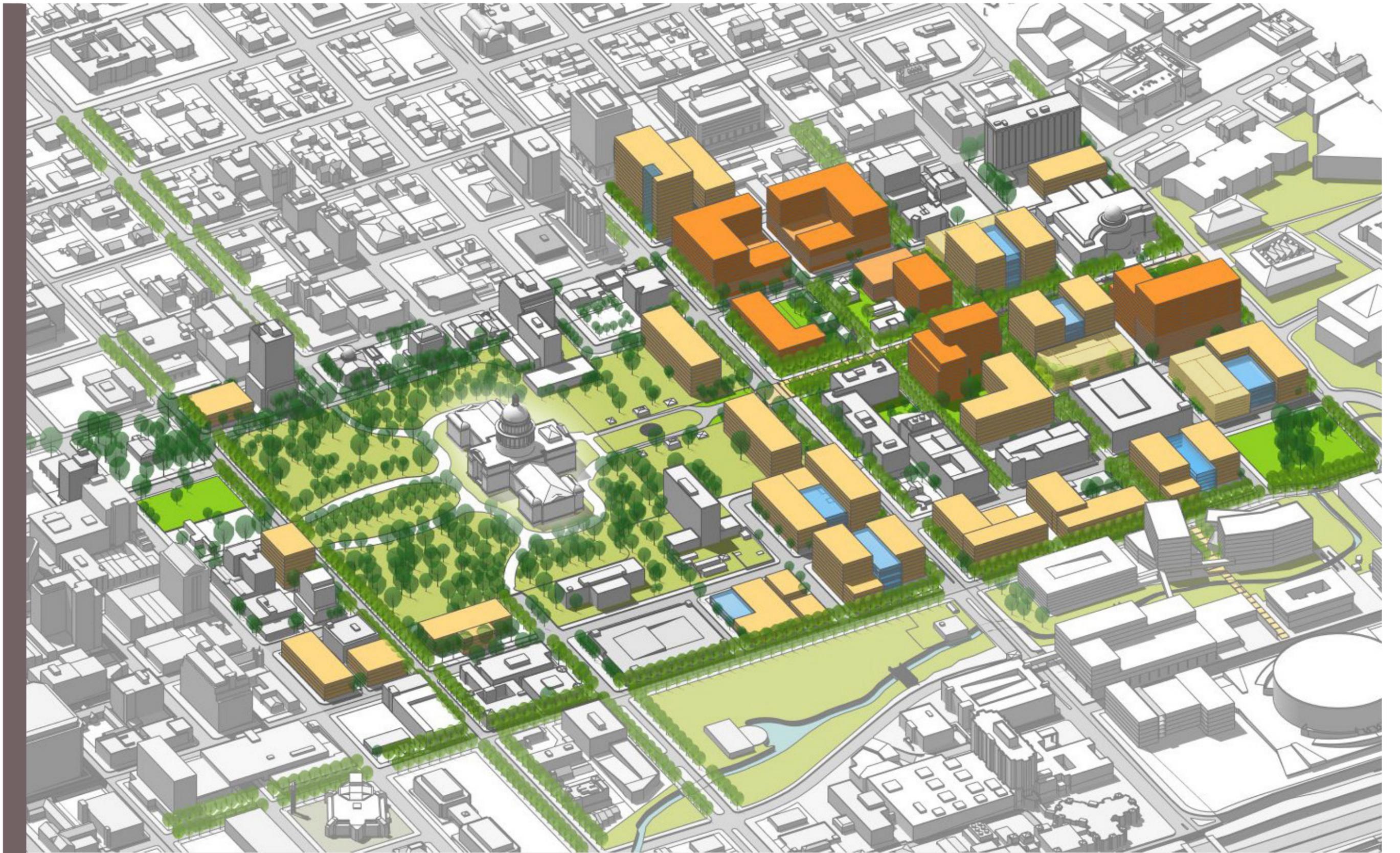


Figure 5.1
Child Care Facility Parking Plan and Section



6

Adjacent Users

6 Adjacent Users

Several existing structures are located along the portion of Congress Avenue that will become the Texas Mall. Of these buildings, three are operated by the Texas Facilities Commission- the Stephen F. Austin Building, the William B. Travis Building, and the Robert E. Johnson Building. Another building, the Capitol District Office Building, which houses DPS, is planned to be demolished as part of Phase 2 development.

The remaining buildings along the future Texas Mall are operated by entities other than the Texas Facilities Commission. These buildings are the Texas State History Museum, run by the State Preservation Board, the historic Gethsemane Lutheran Church, operated by the Texas Historical Commission, and the Chancery, which is owned by the Catholic Diocese of Austin. The Chancery is the only structure along the future Mall that is not owned or operated by any state entity.

The planning team met with each of these adjacent users as part of the study process. These meetings provided a summary of each group's needs or concerns surrounding construction of and relationship to the future Mall. The following notes represent a summary of primary themes discussed with each group.

TEXAS STATE HISTORY MUSEUM

- Strong concern over maintaining Congress Avenue access to the museum at all times for drop-off; two lanes are ideal for vehicles to pass a stopped bus
- Concerns over the impact of the Texas Mall design on the existing museum plaza; the star must remain and existing trees are preferred
- An underground pedestrian tunnel is desired from the new below-grade parking garage to the below-grade museum garage
- Concern over construction activities affecting museum events
- There is some interest in connecting to the Capitol Complex chilled water system in the future

CATHOLIC DIOCESE OF AUSTIN

- There would be interest in purchasing chilled water from the State if this was an option in the future
- Upgraded construction fencing would be preferred so that the front of the building does not look directly into a construction site and excavation
- One-way access to the parking lot is a concern, but this can be solved through the proposed two-way directional modification
- There is a sprinkler system in the front yard and minor electrical lines running to the front steps of the Chancery that should be noted, pending the limits of construction

TEXAS HISTORICAL COMMISSION

- The Gethsemane Church steps must remain intact
- The Gethsemane Church foundation walls are 5-6 feet below grade and are made of rubble limestone; there is concern over future excavation impacting the stability of the foundation if it gets too close; the biggest concerns are drainage, elevation, and vibration
- It would be preferable that the Texas Mall design incorporate a feature, such as a stone band, to indicate the location of existing curbs for Congress Avenue
- Two-way access along 16th Street will be required during construction for access to the THC facilities
- The perimeter garden for the historic buildings is important; the layout dates to 1972; attention should be given to the north side of the Phase 2 15th Street building, which will front on the gardens
- The master plan shows a portion of below-grade parking in Phase 2 that turns north from the primary rectangular parking area at the 15th Street site; it would be preferable to remove that section of parking to create more separation between future excavation and the Gethsemane Church

